

Wilsonville  
**Old Town**

*Neighborhood Plan*

*Adopted September 19, 2011*

**Appendix**

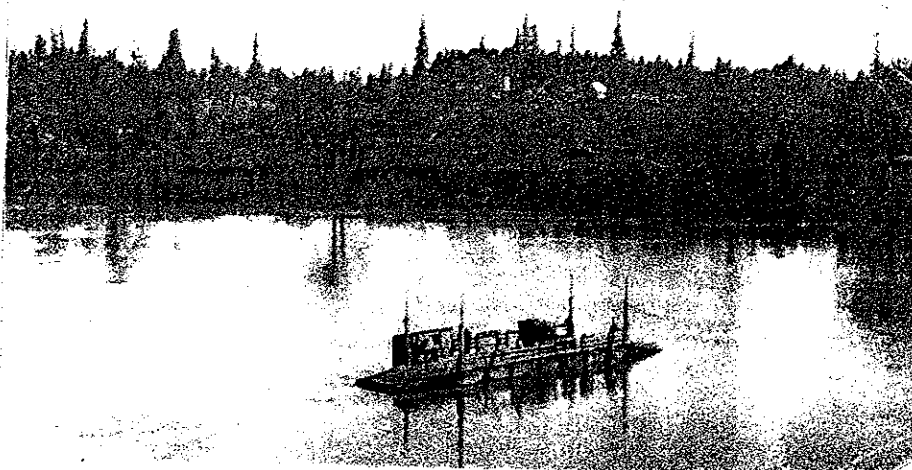
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Appendix A:  
A Short History of Early Wilsonville  
By  
Rose Case

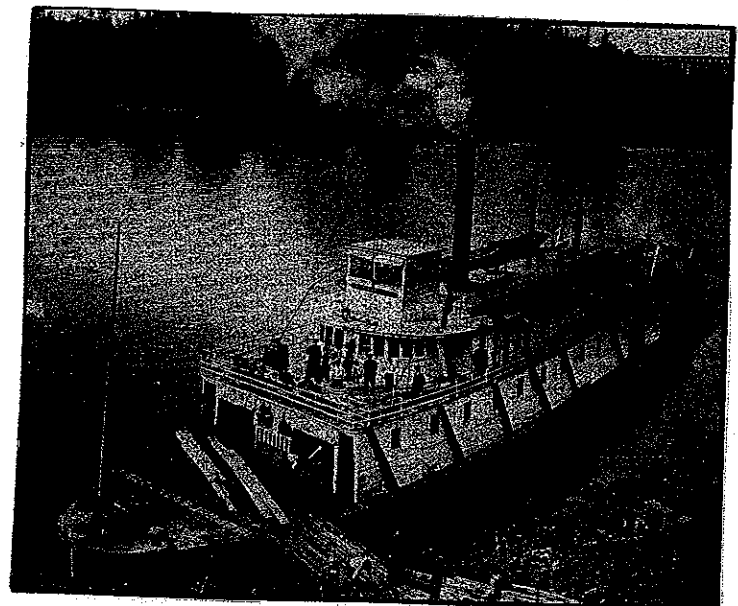


*A SHORT HISTORY OF EARLY WILSONVILLE*

*ROSE C. CASE*



*OREGON'S SOCIAL HISTORY  
HST 240 WINTER TERM  
DICK PINTARICH*



Old Town Neighborhood Plan

Old Town Wilsonville can be found by taking exit 283 from I-5 and traveling West on Wilsonville Road to the first intersection and then turning South on Boones Ferry Road.

This area was originally inhabited by members of the Calapooya Tribe. Although I could find no written documentation of any Calapooya living in this area, my neighbor did find a scaper while working in his garden. By the time the first white settlers were moving into the Willamette Valley, most of the original inhabitants had died from epidemics of small pox, measles and other European diseases.

The Hudson Bay Company Trappers who chose to retire in the Oregon Country by-passed the Wilsonville area and settled about seven miles from here in French Prairie and established the town of Champoeg. The Hudson Bay Company also established a mill at Willamette Falls and Oregon City grew up around it. Wilsonville is located between these two towns on the Willameete River.

It wasn't until 1846 that the first settlers came to this area. Among these early residents were: Robert Valentine Short, Joseph Carey Geer, George Law Curry and Col. Alphonso Boone. These settlers took advantage of the Donation Land Act and chose the Wilsonville area for their new homes.(1)

Robert V. Short's claim encompassed all of the western portion of modern Wilsonville. He became a state legislator and the first assessor for Yamhill County. He surveyed the townsite of Portland and in 1847 helped organize one of the oldest and still active, granges in Oregon, Frogpond. The Joseph C. Geer Farm was West of the Short Farm towards Butteville. His grandson T.T. Geer was the 10th Governor of Oregon.

Col. Alphonso Boone and George Law Curry were members of the first wagon train to travel the Southern Applegate route to Oregon. Curry's land claim was on the South side of the Willamette River. East of I-5 where Charbonneau is located and Boone's claim was right next door to the West of I-5, also on the South side of the river.

Alphonso Boone was the grandson of Daniel Boone. After his wife died, he packed up his 10 children and came to Oregon. Alphonso and his oldest son Jesse established a ferry service across the river. Two years later Alphonso and two of his sons headed south to the California gold field where he died. Jesse ran the ferry for 20 years until his death in 1872.

Jesse offered day and night service. Customers would ring a loud bell to call the ferry. Jesse employed local indians to row the ferry which could carry one wagon at a time. Jesse was The Boone who cut the timber and created a road from Portland to Boones Ferry to Salem. The settlement

Appendix A  
Old Town Neighborhood Plan

which grew next to the ferry crossing was called Boones Ferry.

Alphonso's daughter Chloe Donnelly Boone was one of the first public school teachers in Oregon. In 1848 she married the boy next door, George Law Curry, whom she had met while traveling with the wagon train.

George Law Curry made some important contribution to Oregon. He was editor of the OREGON SPECTOR for a year and then he founded the FREE PRESS in 1848. He served as Provisional legislator, 1848-49; chief clerk of the Council, 1850-51; Member lower house of legislature, 1852-53; Secretary, 1853; Acting Goveenor May 19-Dec. 2, 1853 and Aug.1-Nov. 1, 1854, and then he was appointed Governor, serving until March 3, 1859, when Oregon became a State. Curry County is named in his honor.(2)

Two other resident pioneers made a first in the economic development of the upper Willamette River. John Kruse and Captain Swain built and operated the first commercial steam powered ship above Oregon City.(3)

Kruse and Swain heard there <sup>were</sup> plans to build a steam ship for the upper Willamette River. In May of 1851 they built the HOOSIER. "Then on May 19, 1851, the HOOSIER appeared as the first steam-propelled boat on the upper Willamette. A reconstructed long-boat built below the falls by Captain Swain and Engineer John Kruse, she was portaged to the upper

river and launched at Canemah. Though small and crude in construction, she could carry wheat much faster and cheaper than any flatboat; and she soon put Miller and Pease out of business." (4) Miller and Pease were the major shippers of the upper river. On June 6, 1851 the WASHINGTON came to the upper river and era of river commerce began for the upper valley. By 1928 the steamboat era on the upper Willamette had come to an end. (5)

The period from 1850 through 1900, saw lots of growth for Boones Ferry. Many of the original settlers subdivided their lands and sold them to new settlers. It took only an hour to travel to Portland via steamboat. The main landing was near the 'new' railroad trestle. But there were numerous landings along this section of the river for picking up produce, refueling at woodyards, delivering or taking on mail, cargo and passengers.

In 1860, the first school was built on same site as present day Wilsonville Primary. It went from one room to two rooms by the 1890's. There were 40 students in grades 1 through 8 in 1899 and the school master was paid \$35 a month for Spring and Fall sessions. Older students had to attend high school in West Linn, Tualatin or Portland. There still isn't a High school in Wilsonville.



The 1870's brought settlers from Germany. The Schlitkaiser's came in the late 1870's. His Granddaughter, Eva Moser, remembers that German was still spoken here in the 1920's.(6) In 1878 the German Church was formed. It is still standing and is referred to as the "Frogpond Church".

The Town of Boones Ferry changed names in 1880. It was renamed Wilsonville after Postmaster Charles Wilson. I couldn't find any information about him. The Tauchman house was built on the North side of the Willamette river in 1880. Local folklore has it, that 'old man' Tauchman was the last ferryman before the State took over running it.

Wilsonville continued its quiet existence for two more decades. Wilsonville and Boone's Ferry were on the main road between Portland and Salem. The town was prosperous but remained a small farming community.

In 1908 the Oregon Electric Railroad was built and there was a train depot built here. This railroad ran from Portland to Salem then later on to Eugene.(7) Travel to Portland now took only 45 minutes. The railroad carried flowers from local nurseries and dairy goods to Portland. The town moved Northward along Boones Ferry Road away from the river. Now Wilsonville had the depot, several hotels (one is now a residence), the Farmer's Bank of Wilsonville, drugstore, general store (Henry Aden's), a feed store, a doctor's

office, a saloon, a blacksmith/machine shop and a hardware store.(8)

It wasn't until the end of the 1920's that a paved road or electricity came to Wilsonville. Market Street was built in 1926 between Wilsonville and Oregon City. One side was paved and the other half was dirt. It was referred to as the Scotchman's Road. Today it's Wilsonville Road. Farmers with loaded wagons would use the paved side while empty wagons used the dirt side. 1928 brought electricity to part of Wilsonville. The Young Electric Company was formed and a small generating plant built on Coffee Creek by B.S. Young, "the juice went on at 4 pm everyday, enough to light one bulb in each home." Later the Molalla Electric company brought electricity to the rest of Wilsonville. This area is now served by Portland General Electric.(10)

By 1954, most of I-5 and the freeway bridge were completed. The Boone's Ferry was retired. The Oregon Electric Railway had been bought-out by the Burlington Northern and the depot torn down.

Wilsonville starts its "modern" history. That's another paper.

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Corning, Howard McKinley

(6)(9) BRINGING BACK OLD MEMORIES

Stories from Wilsonville Senior Citizens 1990

3rd Grade Wilsonville Primary

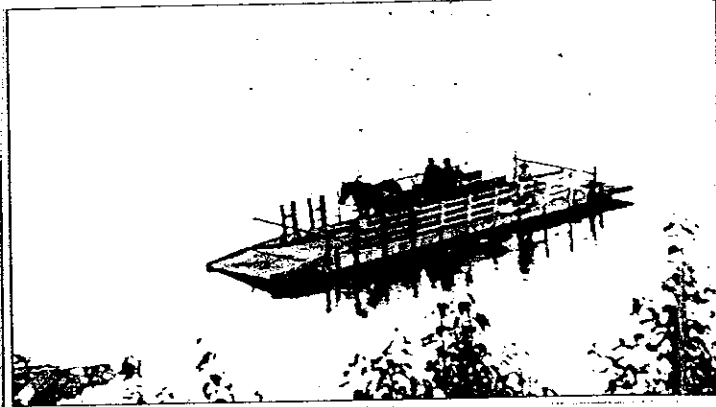
unpublished material

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Edwin D. Culp

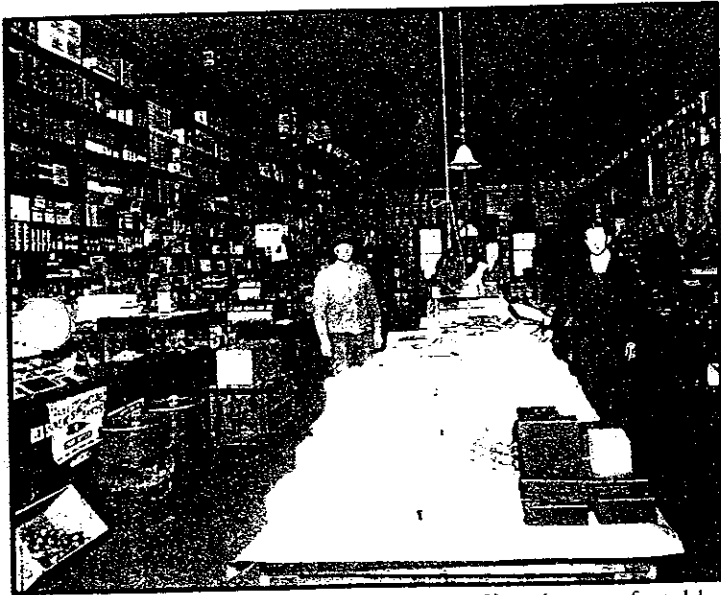
CLACKAMAS COUNTY CULTURAL RESOURCE INVENTORY

Book number VI November 1984

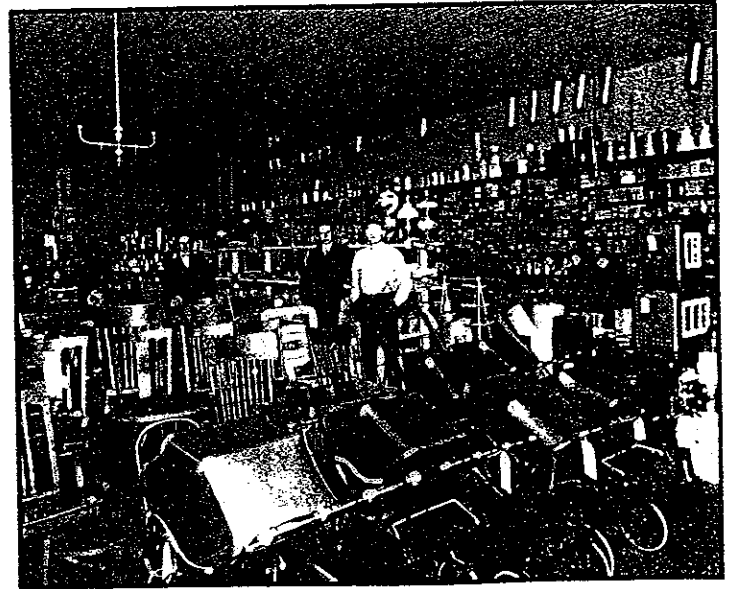


one of the earlier Ferrys

Photos taken from  
 Chamber of Commerce  
 Publication.  
 Emery Aden Collection.



H. D. Aden residence ( ) in 1912 offered a comfortable place to live while operating the Aden General Store, shown here in 1916.



Jake Peters was also an early businessman who operated a hardware store. The Peters residence (cover) was originally built as a boarding house at the ferry landing.



Friends and relatives of early pioneer Gust Jaeger gather for his annual Old Town Neighborhood Plan. His beer was "imported"

on the Oregon Electric Railway, probably from the Blitz Weinhard Brewery in Portland.

Appendix B:

Boones Ferry Historic District

And

Architectural Pattern Book

# BOONES FERRY HISTORIC DISTRICT

*Sustaining Cultural Heritage* &



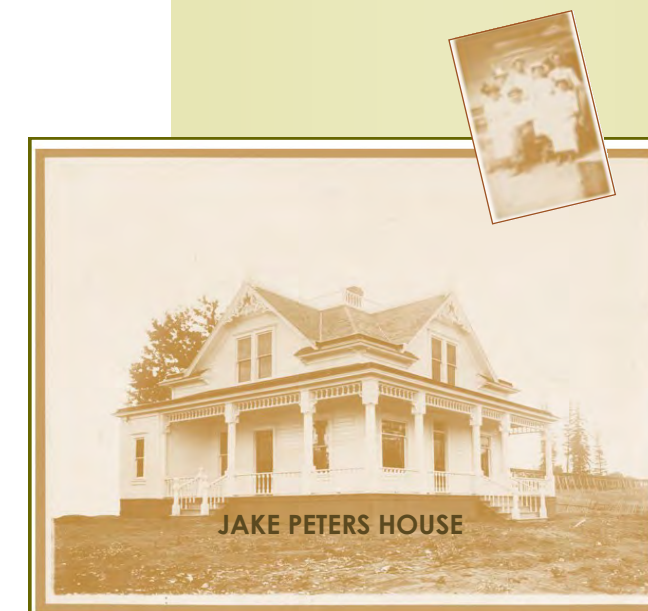
Prepared by Boones Ferry Historic District Neighborhood Association June 2011

BOONES FERRY HISTORIC DISTRICT & ARCHITECTURAL PATTERN BOOK

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# BOONES FERRY Historic District

The Old Town historic area of Wilsonville is a neighborhood where history and the present come together to make this section of Wilsonville what it is today. The anchoring institutions that landmark our neighborhood establish the contributing architectural styles in Old Town.

Building began here about 1850 first in the area around the Boones Ferry Landing. Early buildings were constructed for use as stores, hotels, shops, offices, and banks, with a few homes. Many of the commercial buildings have been converted to residences while maintaining some or all of the architectural characteristics of the original structures.

The architectural pattern book reflects the cultural character of the Boones Ferry historic neighborhood as it has developed over the past 160 years and conserves that culture while embracing thoughtful change. The residents in the proposed Boones Ferry Historic District want to preserve a way of life not found in other Wilsonville neighborhoods by maintaining the existing historic resources and attendant culture as the basis for growth and prosperity in the old town.

The pattern book is presented as a guideline to ensure that proper scale, period reference and culture are preserved. The designs in this pattern book are based on accurate assessments of the historic buildings that remain in the neighborhood. The old town has public garden areas, mature landscapes, dead-end streets, and longtime residents who take pride in their homes and neighborhood. Children can play safely nearly anywhere in the neighborhood.

The purpose of the Old Town Plan and the Boones Ferry Historic District Architectural Pattern Book is to minimize potential future problems with development and redevelopment proposals that are inconsistent with the character and culture of the District. The families who reside here have agreed to have restrictions and conditions in place to preserve the heritage of historic Wilsonville. The Boones Ferry Historic District is truly a defining sector of the character of the community of Wilsonville.



This architectural pattern book is a simple guideline that will help create an urban design that represents the Boones Ferry neighborhood culture. The Boones Ferry community has come together to endorse thoughtful change, while establishing a sense of purpose and permanence for the history represented here. There are currently 12 existing buildings that are approximately 100 years old, all being used as residences. In addition, there are a few public buildings of that same period, i.e. St. Cyril's Catholic Church, the Tauchman House, as well as the old Methodist Church now preserved and included in the Fred Meyer development.

The old town area was built up culturally during several distinct time periods from first platting in 1908, unlike most of Wilsonville which was developed as a series of Planned Unit Developments under master plans which provided common design and development patterns throughout each of the PUDs.

The eclectic nature of old town diversity and architectural variation has resulted in an "affordability" factor. Over the years, families and business owners bought already platted lots and built or redeveloped homes or businesses on them consistent with their goals and resources at that time. The attendant costs of modern development were mostly avoided. Street and sidewalk improvements were generally not required, nor were expensive new infrastructures installed as part of development. The neighborhood developed slowly over the years to the standards of earlier times, creating today's historic character and individuality, typical of many small towns throughout Oregon and the nation.

The State Historic preservation office (SHPO) recognizes buildings that are 50 years as potentially historic consistent with the criteria for listing on the National Register of Historic Places. Historic districts usually have both contributing structures and non-contributing structures. A historic district is an area or neighborhood that has a concentration of buildings and associated landscape - streetscape features (50 years or older) that retains a high proportion of historic character and integrity, representing an important aspect of the city's history.

A contributing property is any building, structure, object, or site within the boundaries of the district that contribute to its historic associations, historic architectural qualities, or archaeological values. It can be any property, structure, or object that adds to the historic integrity or architectural qualities that make the area distinct as being of either local or federal, significance.

Another key aspect of a contributing property is historic integrity. Major alterations to a property can sever its physical connections with the past, lowering its historic integrity. Contributing properties are an integral part of the historic context and character of a historic district, meeting National and State Historic Preservation criteria and qualifying for benefits afforded a property or site listed individually on the National Register.

A non-contributing site has either been so substantially modified that it no longer conveys the historical significance of the district with irreversible modifications or the site was built outside the area's historical timeframe and does not add to the understanding of the district's significance.

The Architectural Pattern Book contains both residential and commercial guidelines that reflect the broad spectrum of cultural diversification that has developed over time in Old Town. Whatever the particular architectural style, buildings in old town are generally simple renditions of that style, with less trim, detail and complexity than more ornate examples of that style. This simplicity is the underlying premise of development and redevelopment in the Boones Ferry Historic District.

## INTRODUCTION

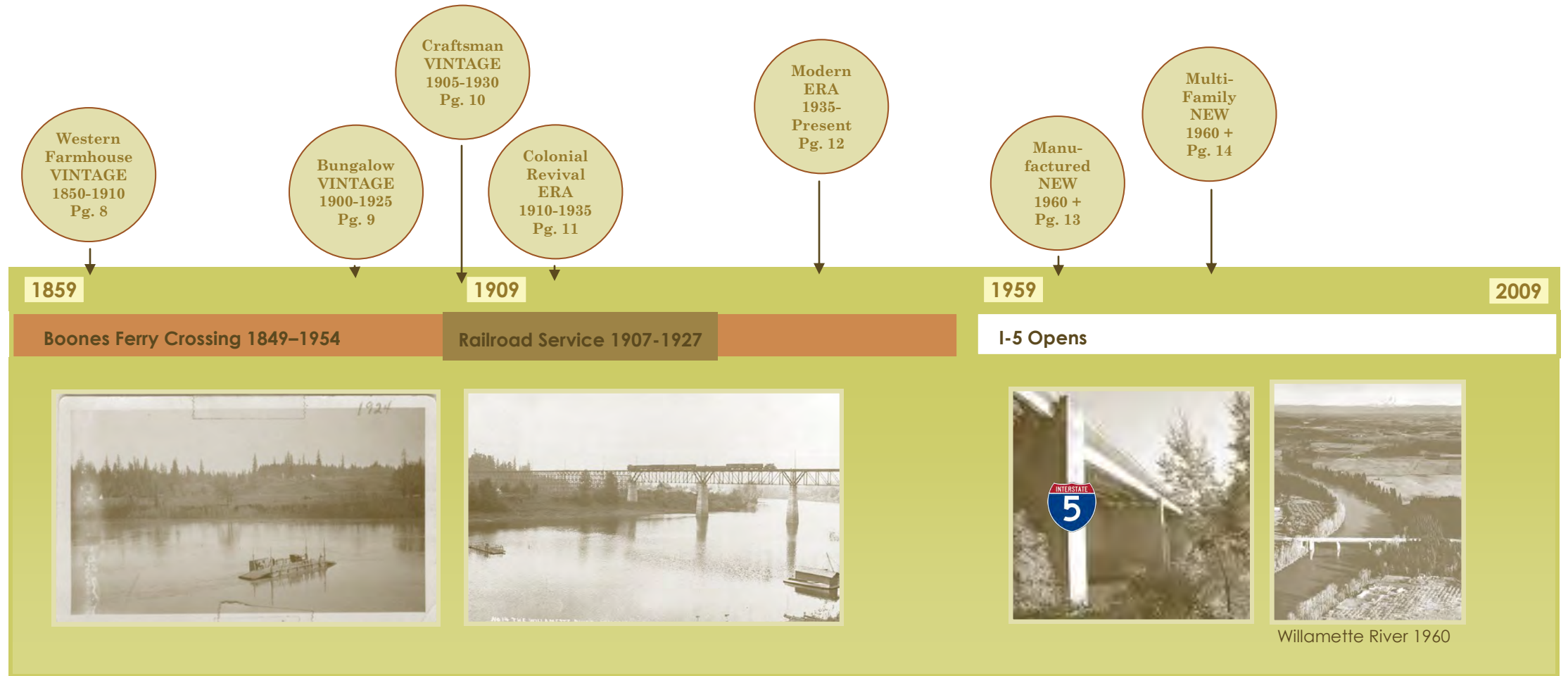


Boones Ferry Historic District

# INTRODUCTION HISTORIC TIMELINE beginnings

## Boones Ferry Historic District

- 1846 Boones Ferry Landing Established
- 1847 Boones Landing begins ferry service
- 1859 Oregon becomes a state
- 1876 Boones Ferry Post Office Established
- 1880 Site renamed Wilsonville by Post Office
- 1890 Old Town held two hotels, a bank, mercantile store, and train depot
- 1905 Cottage Hotel was built  
Norris Young Machine Shop  
Wilsonville Feed Store relocated from river area
- 1906 Railroad tracks and trestle built
- 1908 Oregon Electric Railway service to Salem
- 1909 Emil Tauchman purchased Boones Ferry
- 1910 Commercial Buildings built
- 1911 Wilsonville Farmer's Bank
- 1912 Methodist Episcopal Church
- 1915 Electrical Power arrived



*Old Town Wilsonville developed around the ferry business for over one hundred years. Early travelers were brought across the Willamette by the Kalapuya Indians on canoes around 1850 who followed along an old Indian trail. This corridor is the foundation for the existing Boones Ferry Road, which is the longest continuous road in the Portland Metro Region.*

*The Boones Ferry began service in 1859. Farmers moved their goods to the Portland market, crossing the Willamette on the ferry and passing through the old town, which had a saloon and doctor. When the sternwheelers started up, their captains gathered at a meeting hall that was later moved north to the current center of Old Town along with other buildings around 1905. Other buildings were moved north from*

*the river to Boones Ferry Road and 5th Street in anticipation of the railroad service. The train station brought additional businesses, the bank and hotel were built.*

*The railroad came to Old Town in 1907 with a depot at 5<sup>th</sup> Street and Boones Ferry Road. Development between 1900 and 1930 moved north on Boones Ferry Road to center around the 5<sup>th</sup> and Boones Ferry intersection. Rail passenger service ceased in 1933 and the era of the auto began.*

*With the coming of I-5 in 1954, the Boone Bridge and the I-5 Interchange at Wilsonville Road were built. This moved commercial development toward Wilsonville Road and the freeway ramp near Boones Ferry Road.*

*The incorporation of the City of Wilsonville in 1969 and the development of Town Center, north of Wilsonville Road, east of I-5 gradually moved the center of the community from Old Town to the Town Center area. This move has been further encouraged by the construction of major public buildings, parks and businesses in and near the Town Center area.*

*Stories, and personal memoirs from past periods of development have made a rich cultural history of Wilsonville and made significant contributions to the development of Oregon as a state. Boones Landing and the Boones Ferry served as a major north-south transportation link for commerce and travel between Portland and Salem for 150 years, until the opening of I-5.*

# BOONES FERRY HISTORIC DISTRICT & ARCHITECTURAL PATTERN BOOK

**Vernacular of Old Town Wilsonville**

The Architectural Pattern Book follows historic development patterns by identifying architectural periods consistent with key periods of development. They are:

**VINTAGE 1850-1920**

The period between 1850 and 1925, the era most related to the Boones Ferry and its role in local and regional commerce. Beginning of rail service led to a relocation of the town center of Boones Ferry to Boones ferry Road and 5th street in 1905, renaming the town to Wilsonville after the postmaster in 1880.

**ERA 1920-1960**

Between 1920 and 1960 Road improvements increased auto traffic between Salem and Portland,. Passenger rail service stopped in 1933. The Catholic Church built St. Cyril's Church in anticipation of the priests no longer travelling by rail to hold mass in the small communities. From 1940-1960 Wilsonville was quiet until construction of I-5 with the Boone Bridge and the Wilsonville interchange at Wilsonville Road opened.

**NEW 1960—PRESENT**

The period following the construction of I-5 brought more resident's into Wilsonville with funds made available through the Farm Act.

**Architectural Standards**

Using the Contributing Architectural Styles classification for residential and commercial, it is preferred that all renovation , remodel, and new construction suit the basic styles laid out in this pattern book.

**HISTORIC INTEGRITY RENOVATION**

Exterior changes will maintain architectural integrity of original / existing design and use of materials.

**REMODEL NOT REBUILD**

It is preferred that renovation and remodeling occur instead of new construction wherever possible, maintaining the existing style of the structure as originally intended or as existed at the time of purchase.

If a one story structure requires an additional story to be added, it is preferred that one of the two story designs from the pattern book listed here be the design criteria.

No designs out of scale or massing, using appropriate setbacks, rooflines, and windows

**MANUFACTURED HOMES**

Existing manufactured homes can be replaced by new manufactured homes

**NEW CONSTRUCTION**

Use suggested design styles.

**GENERAL CRITERIA**

**VERNACULAR**

**VINTAGE 1850-1920**

**ERA 1920-1960**

**NEW 1960-PRESENT**



**STANDARDS**

**Historic Integrity Renovation**

**Remodel Not Rebuild**

**New Construction**

## CONTRIBUTING ARCHITECTURAL STYLES

### RESIDENTIAL DESIGN STANDARDS ANCHORING INSTITUTION

Five historic periods of historical architecture have been designated as standards for residential design in the neighborhoods of the Boones Ferry Historic District. CONTRIBUTING

SEE PAGES 7-15



WESTERN FARMHOUSE 1850-1920



BUNGALOW 1900-1925



CRAFTSMAN 1905-1930



COLONIAL REVIVAL 1910-1935



MODERN MIX 1935 -1990

## CONTRIBUTING ARCHITECTURAL STYLES

### NEIGHBORHOOD COMMERCIAL ANCHORING INSTITUTION

Historic commercial buildings and churches are anchoring institutions for Old Town. Their contributing architectural styles set the design standard for neighborhood commercial in the area.

All new construction is to respect the relaxed rural quality that Old Town has always been.

SEE PAGES 17-21



NORRIS YOUNG MACHINE SHOP 1905



ST. CYRIL'S CHURCH 1922



WILSONVILLE FEED STORE relocated 1905



**RESIDENTIAL  
DESIGN  
STANDARDS**

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# RESIDENTIAL DESIGN STANDARDS

## vintage

### WESTERN FARMHOUSE 1850-1920

#### Basic Elements

One, one and a half, or two story  
Side gabled roof, little or no overhang

Boxed Eave

Clad Lap siding or shingles

Prominent Entrance or porch w/ pediment

2-story gable L shape often has continuous porch w/ shed roof at intersection of L

#### Roof

12"-24" max overhang

Roof pitch 7:12—5:12

Gabled Dormers as option

Boxed eave

Optional corbels

#### Windows and Doors

Symmetrical placement of doors and windows

Entrance door located in the center of wide houses, at the corner of narrow houses

Double hung windows with multiple panes



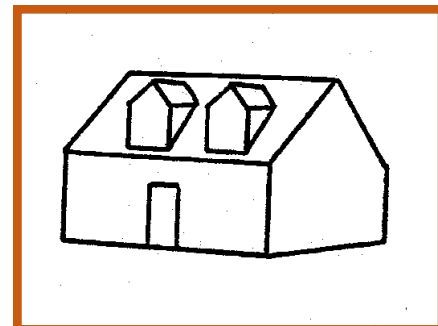
SUGGESTED HOUSE PLAN



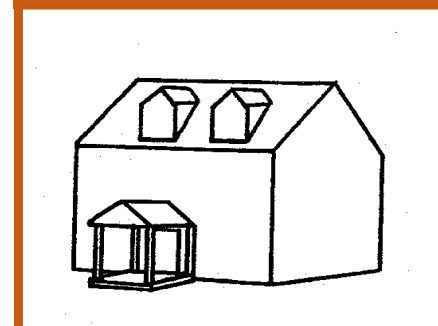
WESTERN FARMHOUSE 1850-1900



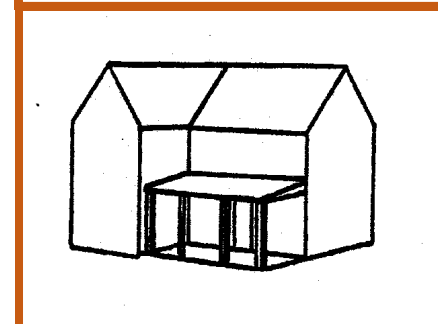
TAUCHMAN HOUSE



SUGGESTED HOUSE PLAN



VINTAGE FARM



VINTAGE FARM



TRAIN MASTERS HOUSE RELOCATED HERE



EX: SRG Homes / JACKSONVILLE, FLA



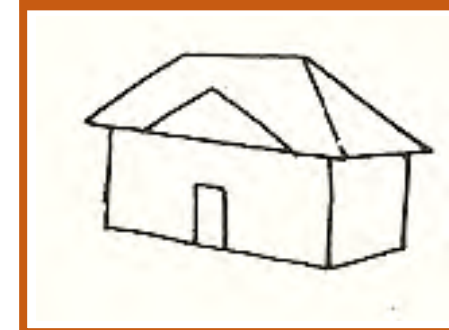
SUGGESTED HOUSE PLAN



OLD TOWN 1910



WILSONVILLE BANK W/ ADDITION



SUGGESTED HOUSE PLAN

# RESIDENTIAL DESIGN STANDARDS vintage

## BUNGALOW 1900-1925

### Basic Elements

- One, one and a half, or two story
- Hip Roof with overhang
- Clad Lap siding or shingles
- Prominent Entrance or porch w/ pediment

### Roof

- 12"-24" max overhang
- Roof pitch 7:12—5:12
- Gabled Dormers as option
- Boxed eave

### Windows and Doors

- Symmetrical placement of doors and windows
- Entrance door located in the center of wide houses, at the corner of narrow houses
- Double hung windows with multiple panes
- Shutters
- Doors

**RESIDENTIAL  
DESIGN  
STANDARDS**  
vintage

**CRAFTSMAN 1905-1930**

**Basic Elements**

One, one and a half, or two story  
Hip Roof with Overhang  
Clad Lap siding or shingles  
Prominent Entrance or porch w/  
pediment

Porches, either full or partial width,  
with roof supported by tapered  
square columns or pedestals fre-  
quently extend to ground level with-  
out break at level of porch floor

**Roof**

12"-24" max overhang  
Roof pitch 7:12—5:12  
Gabled Dormers as option  
Low-pitched, gabled roof ( occa-  
sionally hipped) with wide, unen-  
closed eave overhang  
Roof Rafters usually exposed; false  
decorative beams or braces com-  
monly added under gables,

**Windows and Doors**

Asymmetrical placement of doors  
and windows  
Double hung windows with multi-  
ple panes  
Doors



SUGGESTED HOUSE PLAN



SUGGESTED HOUSE PLAN



ARTS AND CRAFTS 1900-1925



ARTS AND CRAFTS



SUGGESTED HOUSE PLAN



SUGGESTED HOUSE PLAN



SUGGESTED HOUSE PLAN



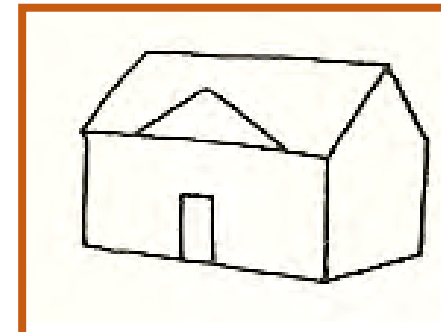
SUGGESTED HOUSE PLAN



SUGGESTED HOUSE PLAN



COLONIAL REVIVAL MOVED HERE 1950



SUGGESTED HOUSE PLAN



SUGGESTED HOUSE PLAN



## RESIDENTIAL DESIGN STANDARDS era

### COLONIAL REVIVAL 1910-1935

#### Basic Elements

One, one and a half, or two story  
Facade is usually symmetrical, but may have side porches or sunrooms on either or both sides.

Rectangular mass

Entrance is centered and accented with columns, pilasters, pediment, and/or hooded to create a covered porch; fanlight or transom, sidelights, paneled door

Wood clapboard most common  
Classical columns, two-story pilasters, dentils under eaves

#### Roof

Usually a side gable roof with narrow eaves, medium pitch, hipped roof and dormers are occasionally seen.

#### Windows and Doors

Multi-pane (six-over-six or six-over-one lights are common), double-hung windows with shutters, bay windows

Palladian windows

# RESIDENTIAL DESIGN STANDARDS

## era

### MODERN MIX 1935-PRESENT

#### BASIC ELEMENTS

Wide eave overhangs, flat roofs, or low-pitched roofs with broad, low, front-facing gables.

Contrasting wall materials and textures,

Unusual window shapes and placements

Shed style. Roof characterized by one or more shed-roofed elements, moderate to high pitch dominate the façade - geometric effect

### RANCH 1960-PRESENT

#### BASIC ELEMENTS

Single story

Low pitched roof with deep-set eaves

Broad rambling facade

Sprawling floor plans

Built long, narrow, and low to the ground

Rectangular, L-shaped, or U-shaped design

Large windows of varying styles

Sliding glass doors leading out to patio and back yard

Attached garage

Emphasis on openness and efficient use of space

Simplicity of architecture



SUGGESTED HOUSE PLAN



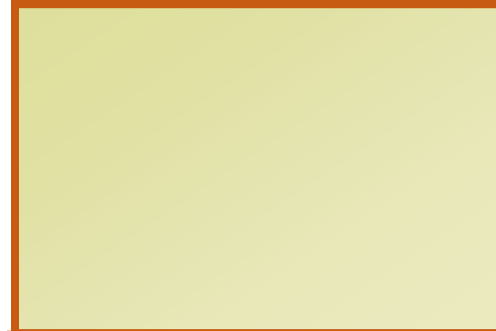
EXISTING RANCH



EXISTING RANCH



EXISTING MODERN MIX



EXISTING RANCH



EXISTING SHED ROOF RANCH



SUGGESTED ENERGY QUALIFIED GREEN HOME



EXISTING MANUFACTURED HOME



SUGGESTED PREFABRICATED DESIGN



EXISTING MANUFACTURED HOME



SUGGESTED ENERGY QUALIFIED GREEN HOME



EXISTING MANUFACTURED HOME

**RESIDENTIAL  
DESIGN  
STANDARDS**  
new

**MANUFACTURED HOUSING  
1960—PRESENT**

Affordable energy efficient modular homes of today resemble residential frame structures and may replace existing manufactured housing in the neighborhood.

A modular home is defined as a building prefabricated and assembled using the same construction methods as stick built homes. Licensed contractors apply the finishes

These homes are available in styles that complement the design standards of Old Town and match the quality of stick built homes.

**RESIDENTIAL  
DESIGN  
STANDARDS**  
new

**MULTI-FAMILY HOUSING 1960**

**1960—PRESENT**

Row Houses

Single Family

Duplex

Street Facing Cottage

Front yard setback: 10'

Side yard setback: 5'

Rear yard setback: 10'

Garage setback: 20'

Building Height: 28'

Private unit green space

Off street parking

These are architectural examples  
of multifamily structures in scale  
with the neighborhood:



EXISTING MULTI-FAMILY 1960



EXISTING MULTI-FAMILY 1960



EXISTING MULTI-FAMILY 1960



MULTI-FAMILY EXAMPLE: CONDOS



EX: STREET FACING FARMSTYLE



EX: STREET FACING COTTAGE

# RESIDENTIAL DESIGN STANDARDS

## NEIGHBORHOOD LAYOUT

### Single Family Residential Lots:

The residential area of Old Town was platted in 1908 with lot sizes:

- 50' x 100' (5000 sq. ft.)
- 50' x 120' (6250 sq. ft.)

The development pattern over the years has honored these original lot sizes, and is the context necessary for historic designation. To remain consistent with historic character maintains the recommended lot size:

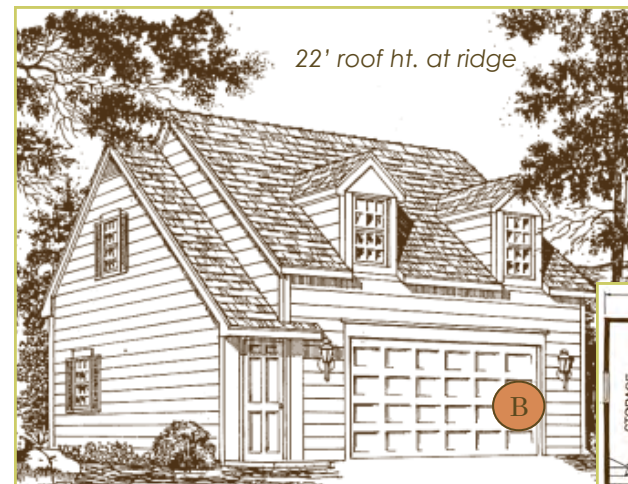
- minimum 5000 sq. ft.
- maximum 6500 sq. ft.

Combining adjacent lots to build larger structures is not in character with historic development pattern and is strongly discouraged

**Ratio of building to lot area is a part of the old town historic character.** Residential building proportional to lot area is generally smaller homes on larger lots. Lot dimensional requirements are to be preserved to maintain existing balance.



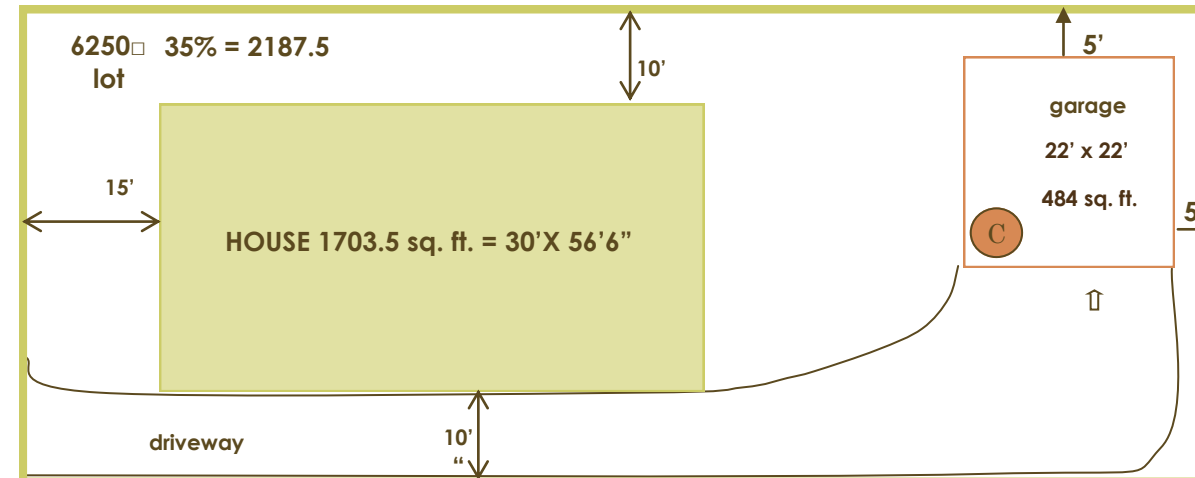
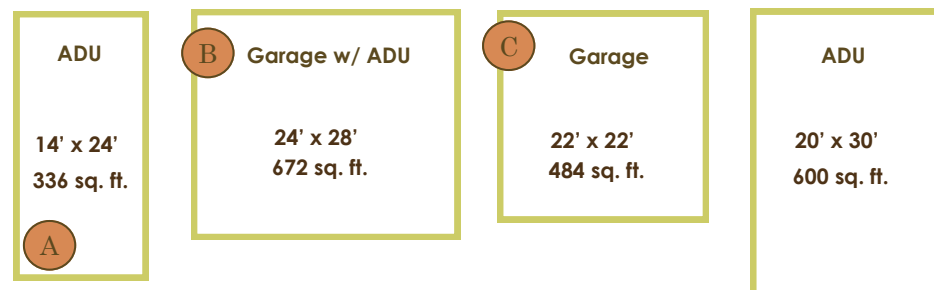
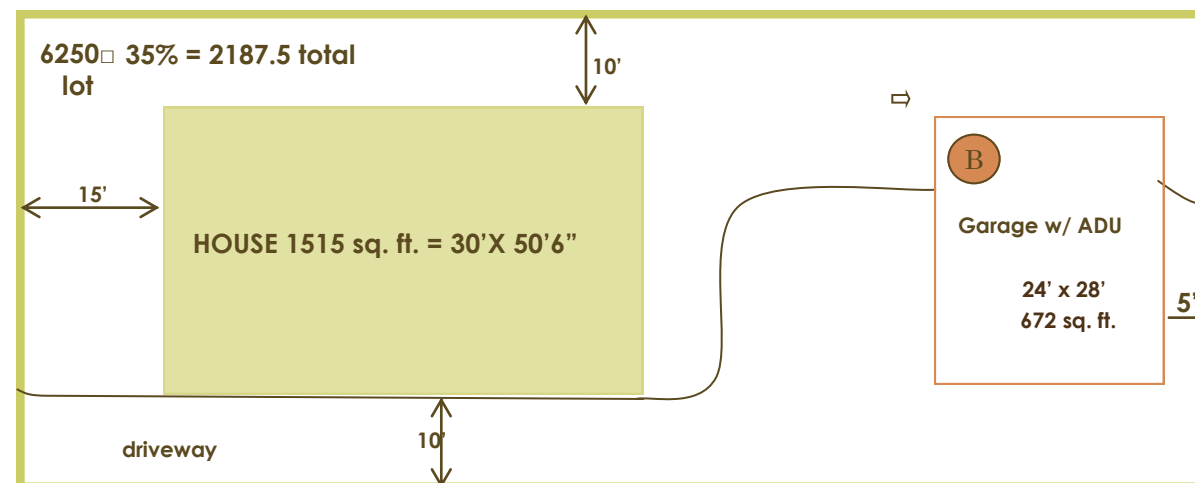
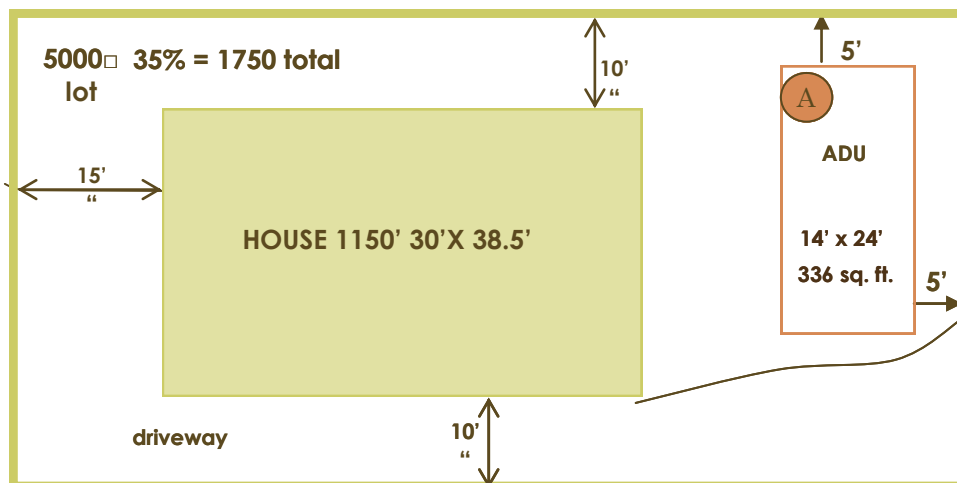
EXAMPLE: 336 sq. ft.



EXAMPLE: 572 sq. ft.



**Average Lot Sizes**  
 5000 sq. ft.  
 6250 sq. ft.  
**Lot coverage maximum:**  
 30% for residential  
 35% all structures



ADU 's are to be no more than 600 square feet and the architecture must be compatible in design and scale with the residential structure on the same lot . Each ADU is to have one off street parking space. The total number is ADU's is to be determined by percentage density as defined by plan.



Q

**NEIGHBORHOOD  
COMMERCIAL**

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# NEIGHBORHOOD COMMERCIAL

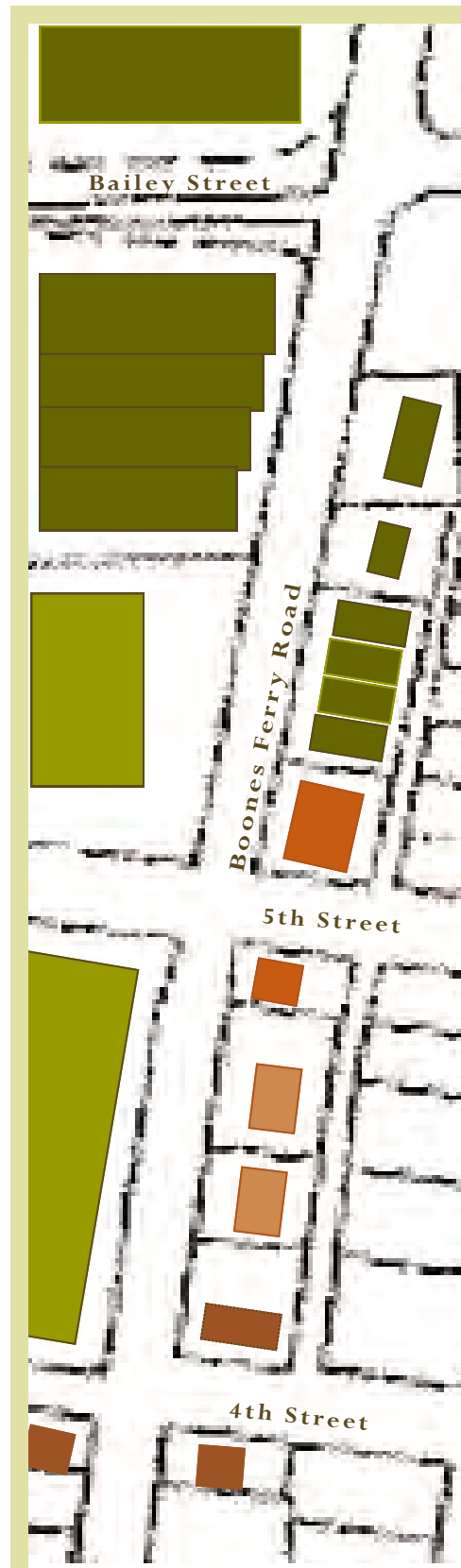
## historic

### BFHD OLD TOWN MAINSTREET

Historic Boones Ferry commercial buildings were shaped and influenced by basic architectural styles of the time and built for purpose. They were all constructed of wood.

As time passed, most of the buildings were remodeled into residences. The original first story front entrance exists in only two buildings. However the archived photos give us a true historical reference to the appearance of the old town commercial area. There was little variation in architectural style except for the craftsman home built by Aden Emery.

- Existing Industrial
- Approved Projects
- Future Redevelopment
- Existing Historic
- Historic Adaptive reuse
- 4th Street Residential



Boones Ferry north from 4th Street 1950

**NEIGHBORHOOD  
COMMERCIAL  
DESIGN STANDARDS**  
examples

**BFHD OLD TOWN MAIN STREET**



# NEIGHBORHOOD COMMERCIAL DESIGN STANDARDS adaptive reuse

## BFHD OLD TOWN MAIN STREET

Maintain authentic historic quality of the structure

Adaptive Reuse Basic Elements  
pg. 21

### Adaptive Reuse

<b>A</b>	Cottage
<b>B</b>	Wilsonville Feed Store
<b>C</b>	Young Machine Shop
<b>D</b>	Aden Store
<b>E</b>	Aden Residence



CURRENT RESIDENCE –COTTAGE 1905



CURRENT RESIDENCE –WILSONVILLE FEED STORE 1918



Adaptive Commercial Use



Adaptive Commercial Use

# NEIGHBORHOOD COMMERCIAL DESIGN STANDARDS adaptive reuse

## BFHD OLD TOWN MAIN STREET

### Basic Elements - Mixed Use

- One and a half or two story
- Side gabled roof, little or no overhang
- Clad Lap siding or shingles
- Prominent Entrance or porch w/ pediment

### Roof

- Roof pitch 7:12—5:12
- Gabled Dormers as option
- Boxed eave
- Optional corbels

### Windows and Doors

- Wood or clad windows
- Symmetrical placement of doors and windows
- Entrance door located in the center
- Double hung wood or clad windows with multiple panes
- Awnings
- Overhangs
- Set-back



CURRENT BUSINESS –YOUNG MACHINE '05



CURRENT RESIDENCE -ADEN STORE 1905



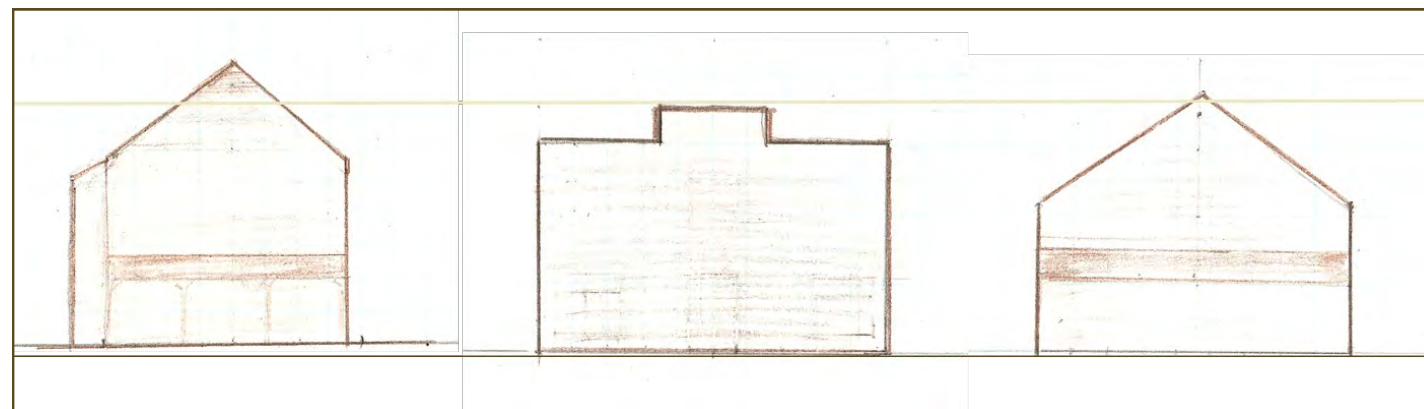
CURRENT RESIDENCE -ADEN HOUSE 1918



Adaptive Commercial Use



Adaptive Commercial Use



SCALE AND MASSING

Maximum building height: 28' 0"

Q

**NEIGHBORHOOD  
MIX**

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# NEIGHBORHOOD MIX

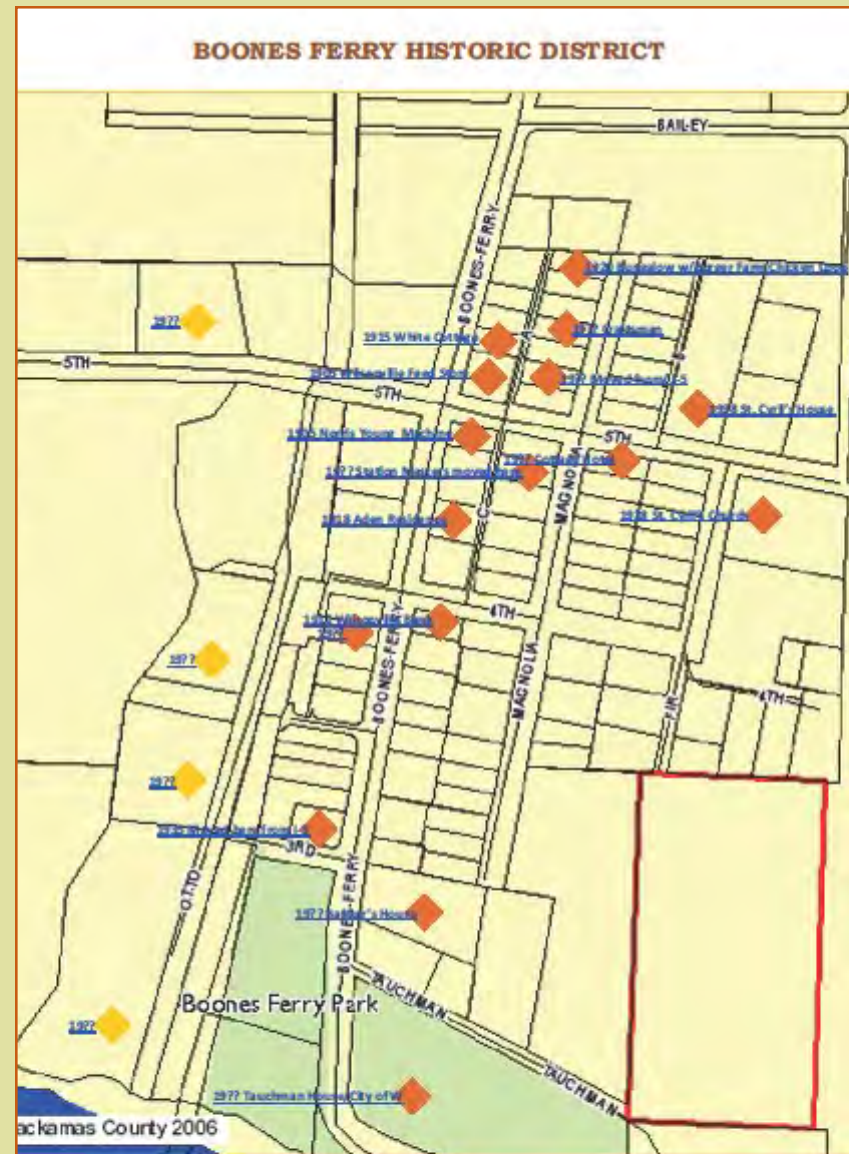
## Historic Inventory

### OLD TOWN AREA

Documentation for the structures listed was started in 1982 by Clackamas County and some are listed in the state inventory

References;  
State Historic Preservation Office

Clackamas County Historic Society



- 1905 - White Cottage
- 1905 - Wilsonville Feed Store
- 1905 - Norris Young Machine Shop
- 1905 - Aden Store
- 19?? - Aden house
- 1911 - Farmers Bank of Wilsonville
- 1910 - Western Farmhouse
- 1935 - Colonial Revival ( moved from I-5 and 5th Street)
- 19?? - Bankers House (Thornton house)
- 1880 - Tauchman House
- 19?? - Station Master House (moved to this location)
- 1910 - Hop House
- 1905 - Cottage Hotel
- 1950's - Brown House – (moved from I-5)
- 1915- Craftsman
- 1920 - Bungalow w/chicken coup
- 1928 - St. Cyril's Catholic Church
- 1943—Parish House (purchase date)

NEIGHBORHOOD  
MIX

Historic Inventory

OLD TOWN WILSONVILLE



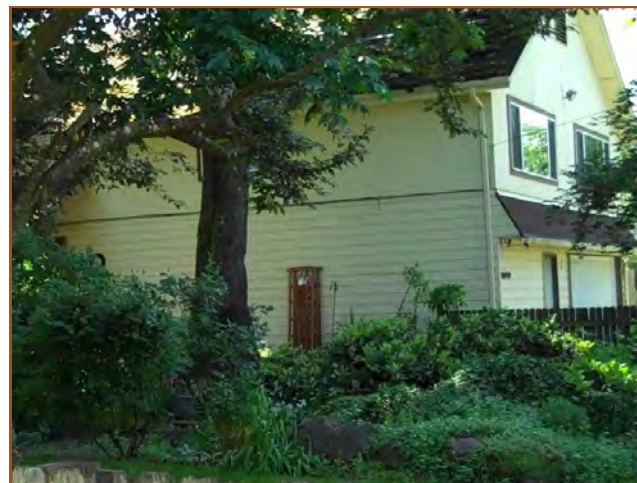
WHITE COTTAGE 1915



WILSONVILLE FEED STORE 1905



NORRIS YOUNG MACHINE SHOP 1905



ADEN STORE 1900



ADEN RESIDENCE 1918



FARMER'S BANK OF WILSONVILLE 1911



WESTERN FARMHOUSE 1910



COLONIAL REVIVAL 1934 (MOVED FROM I-5)



BANKERS HOUSE 1915



TAUCHMAN HOUSE 19???



STATIONMASTER HOUSE 19?? (moved here)



HOP HOUSE 1910



COTTAGE HOTEL 19???



HOUSE MOVED FROM I-5 1950'S



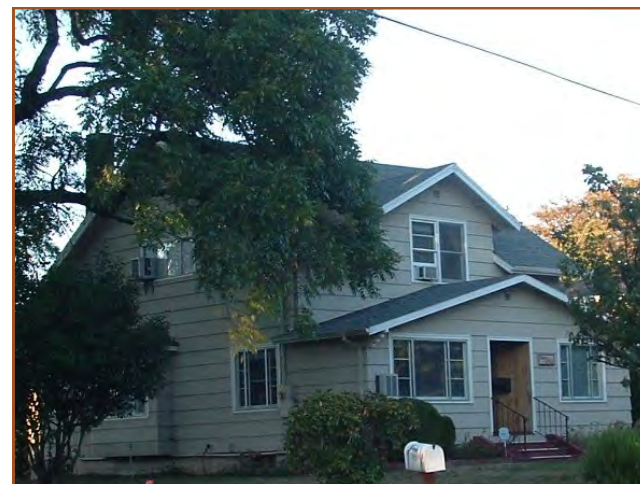
CRAFTSMAN 1915



BUNGALOW W/ CHICKEN COUP 1920



ST. CYRIL'S CHURCH 1928



PARISH HOUSE 1943 (PURCHASE DATE)

**NEIGHBORHOOD MIX**

**Historic inventory**

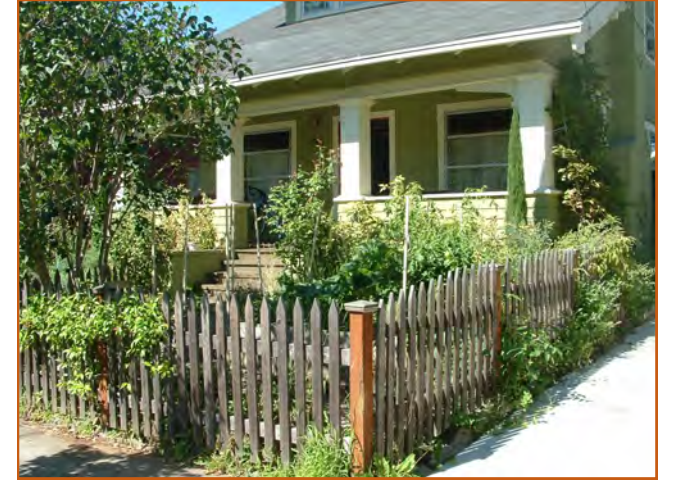
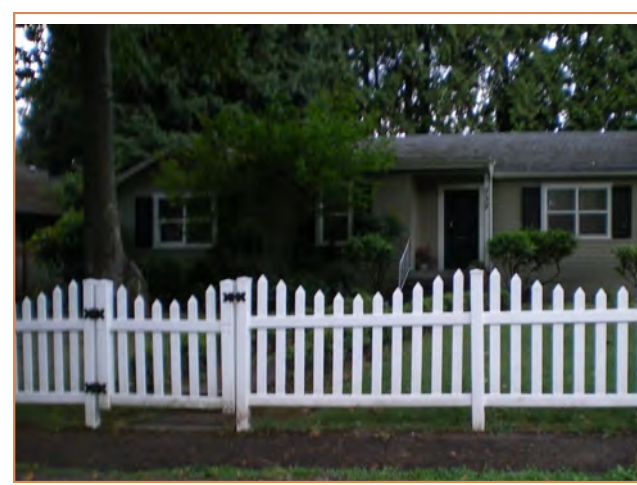
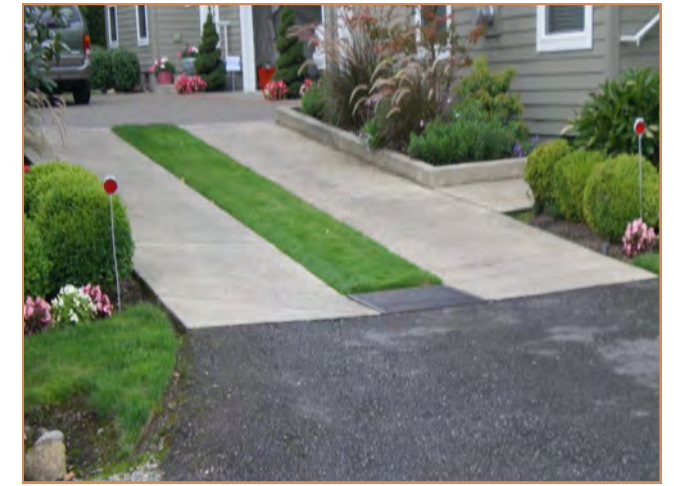
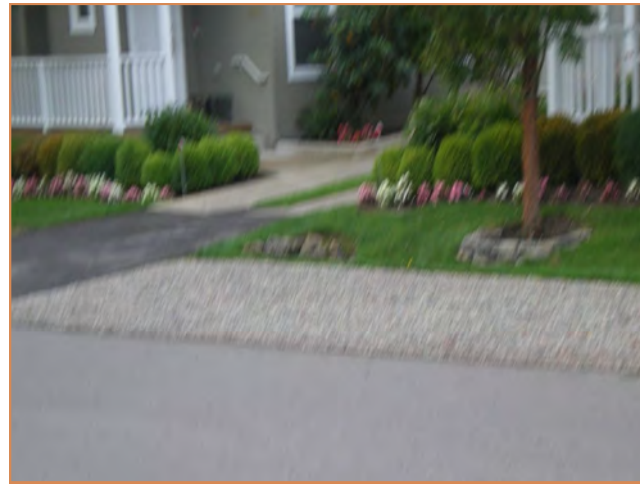
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**OLD TOWN WILSONVILLE**

**BOONES FERRY HISTORIC DISTRICT & ARCHITECTURAL PATTERN BOOK**

# CURBS & SIDEWALKS

## examples



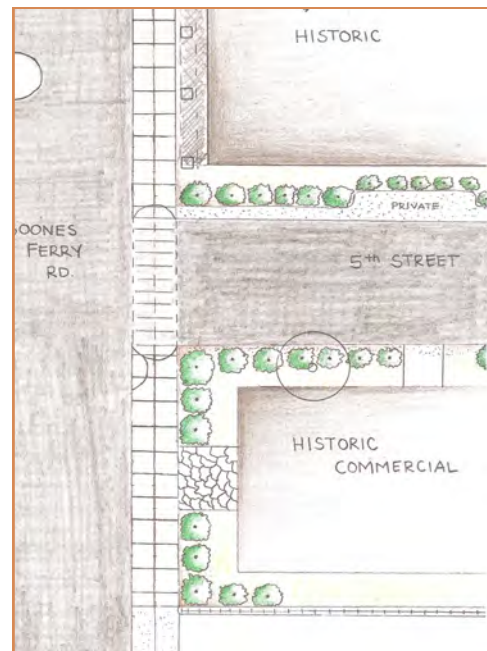
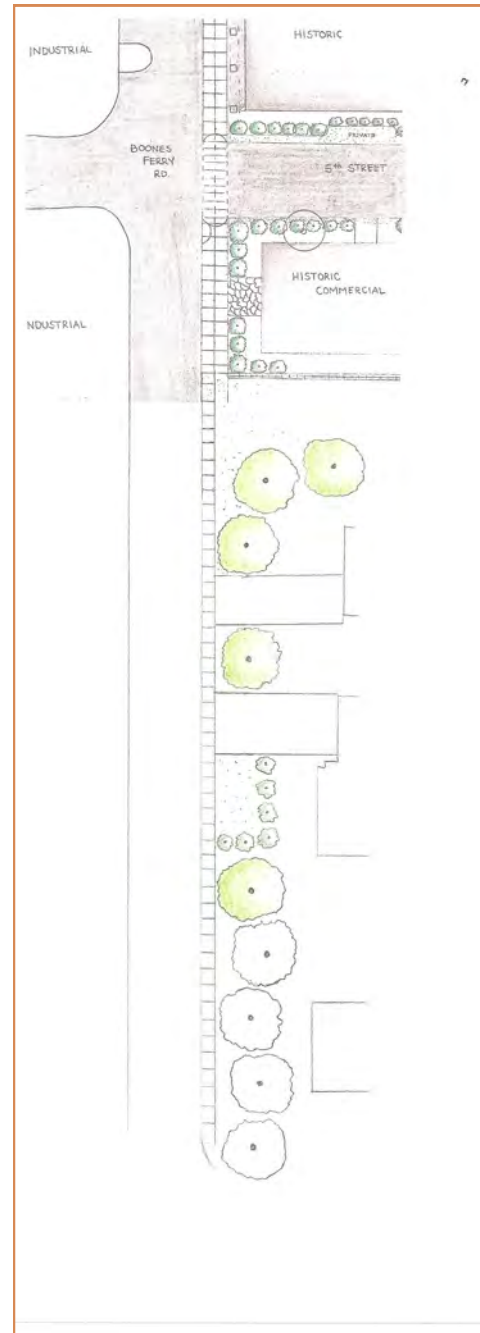
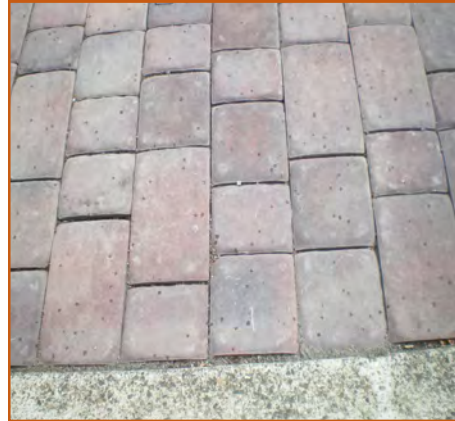
## Curbs and Sidewalks

Grade level paver-like appearance sidewalk on the east side of Boones Ferry Road from Bailey to 5th Streets, no raised bumps or bump-outs.

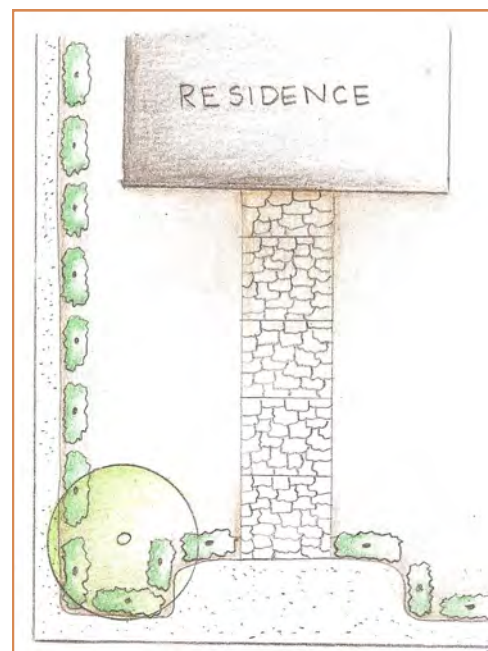
Intersection at eastside Boones Ferry Road & f5th Street continues with grade level paver and sidewalk, no bump out, transitioning into on grade concrete sidewalk.

On-grade sidewalk continues along Boones Ferry road from 4th Street to the river on the east and west sides.

Residential core has 2' gravel strip as standard rather than curb and sidewalk. Homeowner can designate up to an additional 6' gravel area for personal parking. Between yard and gravel requires edging of fence, hedge, landscaping.



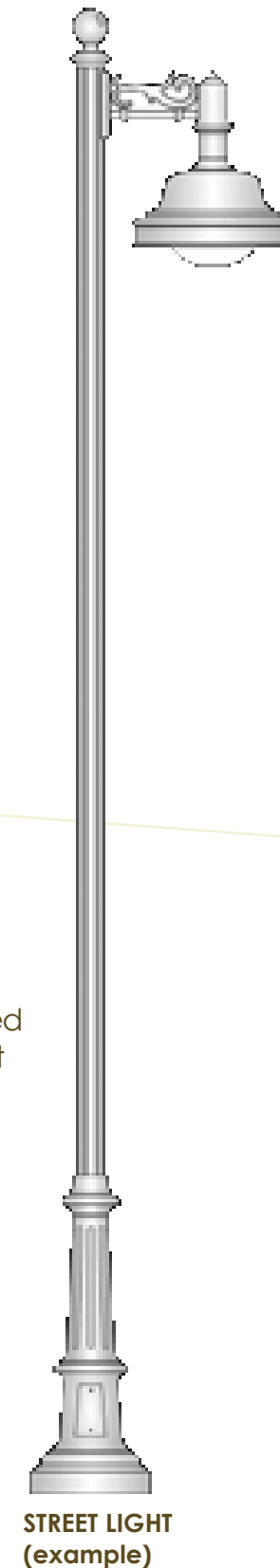
**BOONES FERRY & 5TH  
SIDEWALK & INTERSECTION**



**RESIDENTIAL  
PULL OFF PARKING**

## Lighting

The lighting standard for BFHD maintains canopied lighting throughout the neighborhood commercial and residential.



**STREET LIGHT  
(example)**

## CURBS, SIDEWALKS, LIGHTING



## OLD TOWN NEIGHBORHOOD VISTAS

The open vistas are an important characteristic of Old Town Wilsonville. They are an integral part of what makes Old Town contrast other neighborhoods. Many old established heritage trees throughout the neighborhood create these vistas which are actual places for children to play and neighbors to walk and gather.

The vistas of Old town are another cultural aspect to be preserved.



**WILSONVILLE  
AREA**

**Heritage Locations**

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Q



Odd Fellows Meeting Hall



Old Methodist Church



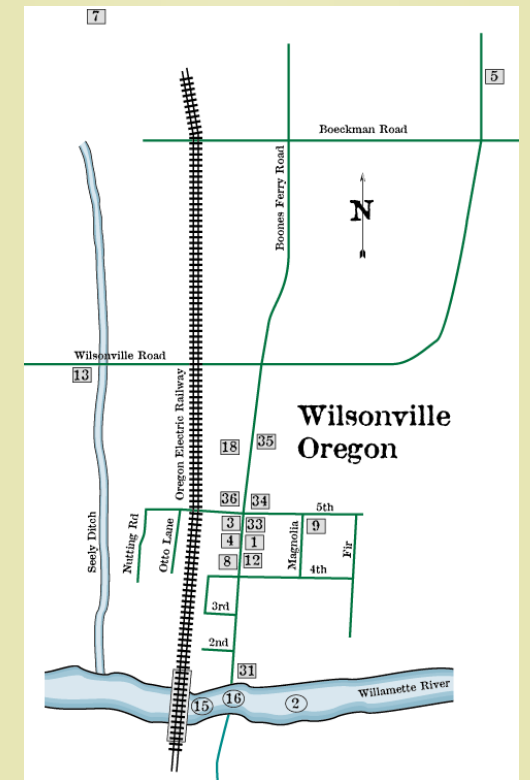
Frog Pond School



## WILSONVILLE AREA

### heritage locations

Many historic structures and land areas have not survived the development of Wilsonville, but their stories live on through the stewardship of the Boones Ferry - Wilsonville Historical Society



Wilsonville Historical Tour Map

Wilsonville-Boones Ferry  
Historical Society

# BOONES FERRY HISTORIC DISTRICT & ARCHITECTURAL PATTERN BOOK

## GLOSSARY

### A

**Adaptive re-use:** a use for a structure or landscape other than its historic use, normally entailing some modification of the structure or landscape.

**Adjacent:** See adjoining.

**Adjoining:** Contiguous or abutting exclusive of street width. It shall include the terms adjacent, abutting or contiguous.

**Alley:** A minor access way which is used to provide vehicular ingress and egress to the back or side of properties otherwise abutting on a street. An alley typically has a width of no more than twenty (20) feet.

**Anchoring Institutions** the places where the cultural, educational, and social activities of community are focused. Their continuing presence preserves cohesive neighborhoods. The essence of an anchor is that it answers community needs, serves a variety of social functions and is structurally prominent without necessarily being monumental. The physical form of the institution recalls its original role and reminds people of the neighborhood's cultural heritage. The landmark conveys a sense of permanence and continuity.

**Architectural Character:** The distinctive qualities of the form, features, details, color and ornamentation that comprise a style of building.

**Architectural conservation:** the science of preserving a historic structure's materials by observing and analyzing their deterioration, determining causes of and solutions to problems, and directing remedial interventions.

**Building:** an enclosed structure with walls and a roof, created to serve some residential, industrial, commercial, agricultural, or other human use.

**Building Façade:** The exterior elevation(s) of a building; usually set parallel to the front lot line, often distinguished by elaboration of architectural characteristics.

**Building Façade, Primary:** The main exterior elevation of a building; usually associated with its primary entrance and/or street address.

**Building or Structure Height:** The term 'height of building or structure' shall be deemed to mean the perpendicular distance from the average elevation of the adjoining ground to the highest point of the middle height gable between the eaves and ridge of a pitch or hip roof.

### C

**Character-defining feature:** a prominent or distinctive aspect, quality, or characteristic of a historic property that contributes significantly to its physical character. i.e., structures, objects, vegetation, spatial relationships, views, furnishings, decorative details, and materials

**Commercial:** Development having to do with retail, service, commercial recreation, and/or office uses.

**Conservation district:** Locally designated areas, in which regulations for alteration or removal apply only to specific historic buildings within the boundary.

**Cultural landscape:** a geographic area, including both cultural and natural resources and the wildlife or domestic animals therein, associated with a historic event, activity, or person or exhibiting other cultural or aesthetic values. There are four general kinds of cultural landscape, not mutually exclusive.

**Cultural resource:** an aspect of a cultural system that is valued by or significantly representative of a culture or that contains significant information about a culture. A cultural resource may be a tangible entity or a cultural practice.

### D

**Density:** The number of residential units per acre of land.

**Design:** the combination of elements that create the form, plan, space, structure, and style of a historic property.

**Design intent:** the creative objectives of a designer, architect, landscape architect, engineer, or artist that were applied to the development of a historic property.

### F

**Feature (historic):** (1) a prominent or distinctive aspect, quality, or characteristic of a historic property; (2) a historic property.

**Feeling (historic):** a property's expression of the aesthetic or historic sense of a particular period of time.

### H

**Historic character:** the sum of all visual aspects, features, materials, and spaces associated with a property's history.

**Historical context:** an organizing structure created for planning purposes that groups information about historic properties based on common themes, time periods, and geographical areas

**Historic designed landscape:** a landscape significant as a design or work of art; was consciously designed and laid out either by a professional or amateur according to a recognized style or tradition; has a historical association with a significant person, trend or movement in landscape gardening or architecture, or a significant relationship to the theory or practice of landscape architecture.

**Historic district:** a local or national geographically definable area, urban or rural possessing a significant concentration, linkage, or continuity of sites, landscapes, structures, or objects, united by past events or aesthetically by plan or physical developments. A district may also be composed of individual elements separated geographically but linked by association or history. (See National Register Bulletin 15.)

**Historic property:** a district, site, structure, or landscape significant in American history, architecture, engineering, archeology, or culture; an umbrella term for all entries in the National Register of Historic Places.

**Historic significance:** the meaning or value ascribed to a structure, landscape, object, or site based on the National Register criteria for evaluation. It normally stems from a combination of association and integrity.

**Historic site:** a landscape significant for its association with a historic event, activity, or person. (City of Wilsonville)

**Historic site:** the site of a significant event, prehistoric or historic occupation or activity, or structure or landscape whether extant or vanished, where the site itself possesses historical, cultural, or archaeological value apart from the value of any existing structure or landscape; see cultural landscape. (Architectural Heritage Center, Portland, Oregon)

### I

**Integrity:** the authenticity of a property's historic identity, evidenced by the survival of physical characteristics that existed during its historic or prehistoric period; the **extent to which a property retains its historic appearance.**

**Inventory:** a list of cultural resources, usually of a given type and/or in a given area

### L

**Lot Coverage:** The area of a lot covered by all of the buildings on that lot, expressed as a percentage of the total lot area.

**Lot Depth:** The lot depth is the mean average distance between the front lot line and rear lot line of a lot measured within the lot boundaries.

**Lot, Front:** The boundary line of a lot abutting a street, other than a boundary line along a side or rear yard. If the lot does not abut a street, the narrowest boundary line shall be considered to be the front.

**Lot Line, Front:** The boundary line of a lot abutting a street, other than a boundary line along a side or rear yard. If the lot does not abut a street, the narrowest boundary line shall be considered to be the front.

**Lot, Width:** The 'lot width' is the mean horizontal distance between the side lot lines of a lot measured within the lot boundaries.

**M**

**Manufactured Homes** – homes built inside a factory to a federal code and inspected by state agencies which are federally certified (HUD). (Oregon Manufactured Home Association)

**Material:** the physical elements that were combined or deposited to form a property. Historic material or historic fabric is that from a historically significant period, as opposed to material used to maintain or restore a property following its historic period(s).

**Mixed Use:** A development in which a site or building provides more than one type of use, such as commercial and residential

**Modular Home:** A structure intended for residential use that has sleeping, cooking and plumbing facilities and is constructed off-site in compliance with the Uniform Building Code (Oregon State Structural Specialty Code) and designed to be transported to a site for installation and/or assembly of modular components to form a permanent structure.

**N**

**National Historic Landmark:** a district, site, building, structure, or object of national historical significance, designated by the Secretary of the Interior under authority of the Historic Sites Act of 1935 and entered in the National Register of Historic Places.

**National Register of Historic Places:** the comprehensive list of districts, sites, buildings, structures, and objects of national, regional, state, and local significance in American history, architecture, archeology, engineering, and culture kept by the NPS under authority of the National Historic Preservation Act of 1966.

**N**

**National Historic Landmark:** a district, site, building, structure, or object of national historical significance, designated by the Secretary of the Interior under authority of the Historic Sites Act of 1935 and entered in the National Register of Historic Places.

**National Register of Historic Places:** the comprehensive list of districts, sites, buildings, structures, and objects of national, regional, state, and local significance in American history, architecture, archeology, engineering, and culture kept by the NPS under authority of the National Historic Preservation Act of 1966.

**Neighborhood: An urban sector of multiple uses served by a network of pedestrian friendly streets and alleys** within approximately ¼ mile in radius. Neighborhoods are defined by arterial or collector streets and/or open space at their edges and include a Neighborhood Commons at their center.

**O**

**Office Complex:** A planned development commercial center or complex of administrative, professional and general office uses. Typical uses include governmental, financial, architectural, medical, dental, legal, real estate, accounting, insurance and general business offices.

**Ornamentation:** 167. The details of shape, texture, and color that are deliberately added to a structure for decorative effect.

**P**

**Parking Space:** A permanently surfaced and marked area not less than nine (9) feet wide and eighteen (18) feet long, excluding paved area necessary for access, for the parking of a motor vehicle.

**Pattern Book:** An illustrative document that depicts the architectural character of a proposed development, in compliance with the Design Principles and Design Standards.

**Pedestrian Access:** A path of travel to approach and enter a building, or open space on foot.

**Period of significance:** the span of time in which a property attained the significance for which it meets the National Register criteria.

**Pattern Book:** An illustrative document that depicts the architectural character of a proposed development, in compliance with the Design Principles and Design Standards

**Period of significance:** the span of time in which a property attained the significance for which it meets the National Register criteria.

**Porch:** An open-air room appended to the mass of a building, with floor and roof.

**Preservation:** the act or process of applying measures to sustain the existing form, integrity, an material of a historic structure, landscape or object.

**Row House:** A form of single family dwelling where individual units are arranged in an unbroken row with no intervening side yards, with or without an accessory dwelling unit or building.

**S**

**Secretary of the Interior Standards:** See [http://www.nps.gov/history/local-law/arch\\_stnds\\_8\\_2.htm](http://www.nps.gov/history/local-law/arch_stnds_8_2.htm)

**Setback:** The distance between a reference line (usually a property line) and the nearest point of a building, or portion thereof. In the Village Zone, see Section 4.125.06, Standards Applying to all Developments.

**Setting:** the physical environment of a historic property; the character of the place in which the property played its historical role.

**State historic preservation officer (SHPO):** an official within each state appointed by the governor to administer the state historic preservation program

**Street, Collector:** A street used to some extent for through traffic and to some extent for access to abutting properties.

**Street, Local:** A street used exclusively for access to abutting properties.

**Structure:** a constructed work, usually immovable by nature or design, consciously created to serve some human activity.

**V**

**Vista:** A view seen through a long avenue between trees or houses.

**Y**

**Yard:** The open space between a structure or structures and any lot line.

Q



**WILSONVILLE, OREGON**  
Ca. 1910

This intarsia mural was commissioned by the Wilsonville Public library Foundation and constructed at the Oregon State Correctional Institution in 2003. It contains over 5000 pieces of 57 different woods, all in their natural colors.

Appendix C:  
Boones Ferry Road Streetscape Project

By

Nevue Ngan Associates

A technical memorandum prepared February 2008







# BOONES FERRY ROAD STREETScape PROJECT



## DESIGN GUIDELINES FOR BOONES FERRY ROAD IN OLD TOWN WILSONVILLE

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**TECHNICAL MEMORANDUM  
FEBRUARY 2008**

**CITY OF WILSONVILLE**

PREPARED BY  
NEVUE NGAN ASSOCIATES



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## Purpose

The purpose of this study was to develop a street right-of-way plan for Boones Ferry Road in the Old Town district of Wilsonville. This memo records the inventory, design concepts, public process, and recommendations that were made as part of this study. Additional illustrations and design concepts that were produced for this project are included in a power point presentation and attached in CD format in Appendix I.

The Boones Ferry Road Streetscape Project grew from the Old Town Neighborhood Plan. The Old Town Plan (June 2007) was based on a series of meetings with Old Town residents and business owners. The purpose of that plan was to identify the essence of what makes Old Town a special place and to provide direction to future development and redevelopment in the Old Town community.

## Project Goals

Goals of the project were defined at the beginning of the project and refined through the planning process. Goals of the project are as follows:

- Develop general street standards for Boones Ferry Road from Wilsonville Road to Boones Ferry Park
- Respect history of road and district and create standards that highlight historic identity
- Support businesses and respect homeowners
- Develop standards that are consistent with other city planning efforts
- Review existing street widths, bicycle lanes, walks, plantings, lighting, and furnishings to inform new standards
- Develop standards that are appropriate for various conditions along the street
- Make right-of-way development compatible with city engineering standards
- Investigate ways to use low technology and green solutions for stormwater treatment



*New Commercial Development on Boones Ferry Road*

## Public Involvement

Public involvement played a key role in the design process for Boones Ferry Road Streetscape Project.

Three public meetings were held to discuss the project and gain public input from neighborhood residents and stakeholders.

Meetings were held on the following dates: September 16, October 29, and December 6. Options that addressed street issues and development alternatives were presented to neighbors. During each meeting, attendees reviewed the design proposals and divided into small discussion groups to comment on the plans. Public comments and concerns were recorded and incorporated into the streetscape standard concepts. Information and proposals from the meetings were posted on-line on the city website.

General development concept and recommendations are informed by public commentary, site conditions, and existing planning documents. A complete list of the public comments is included at the end of this report in Appendix II.

# THE SITE



## The Site

Boones Ferry Road marks the western edge of Wilsonville’s Old Town district, where the town was originally settled. The road is a historic route between the Willamette Valley and Portland. Until construction of Interstate 5 and development patterns in the city began to change, Boones Ferry Road served as the town’s main street.

Within Old Town, the character and use of Boones Ferry Road changes as it moves from Wilsonville Road toward the river.

At the district’s northern edge from Wilsonville Road to Bailey Street, large-scale commercial development exists or is planned. From Bailey Street to Fourth Street, development patterns are mixed and include apartments, commercial, and single family residential. From Fourth Street to Second Street, development is residential. From Second Street, the road dead ends at Boones Ferry Park, where the historic ferry landing exists at the Willamette River.

Aerial Photograph of Boones Ferry Road

### Existing Conditions

Right-of-way improvements vary from Wilsonville Road to the Park. Traffic lane widths, sidewalks, bike lanes, and on-street parking respond to historic use patterns and city standards at the time a particular site was developed. General improvements are shown on the adjacent illustration, Figure I. The illustration also shows how the use of the street and the scale of the development changes from north to south. The general land use divisions and street elements are indicated by the colors shown.

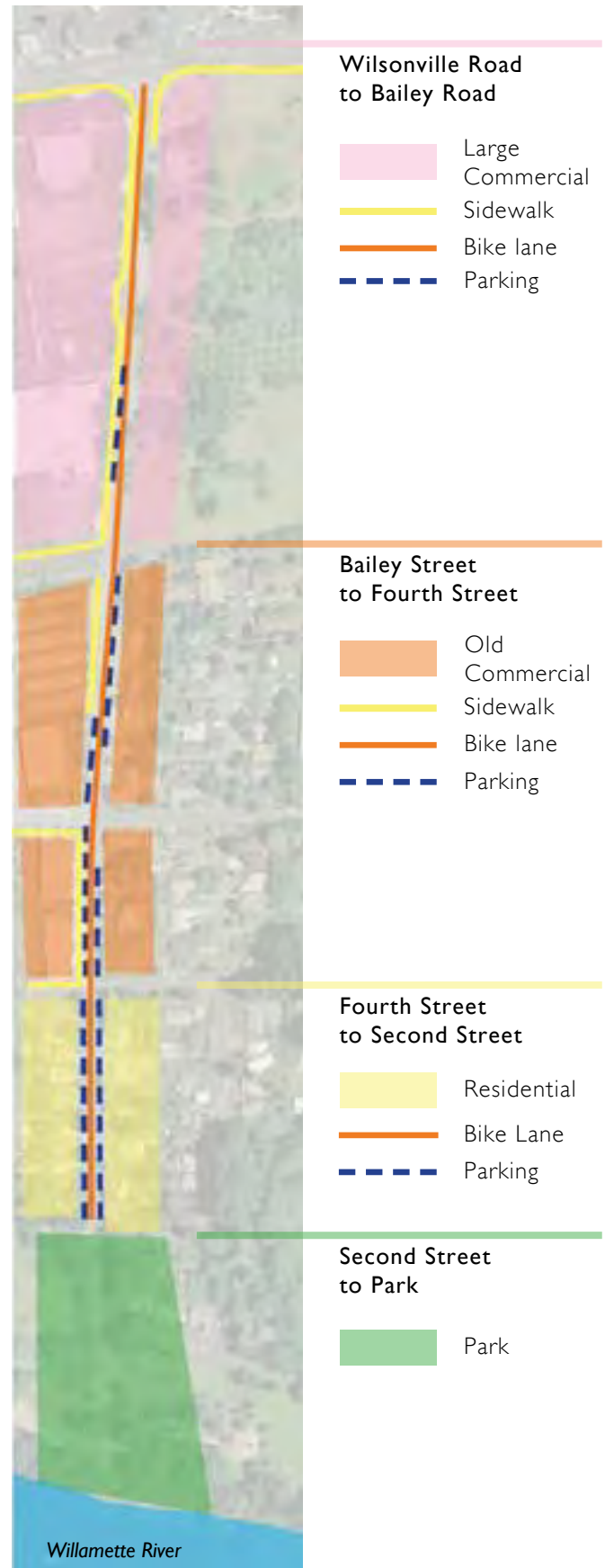


Figure I: Existing Conditions at Boones Ferry Road

## STREET HISTORY



Boones Ferry Road, 1900



Boones Ferry Road, looking north, "Main Street," 1910



Boones Ferry Road, 1950



Boones Ferry Road, 1950

## Street History

Boones Ferry Road history begins in the 1840s when Col. Alphonso Boone, (grandson of Daniel Boone), moved his family by wagon trail along the Applegate Trail to Willamette Valley settlements. In the spring of 1847, the Boones family claimed 1,000 acres at current day Charbonneau, built a ferry and cleared a road along an Indian trail. The establishment of Boones Ferry and Boones Ferry Road enabled 24-hour commerce between Salem and Portland. Boones Ferry operated from 1847-1959 when it was made obsolete by the construction of Interstate 5.

During the past 150 years, Boones Ferry Road has experienced change. The Boone Family widened and improved the path by laying down a "corduroy road" of split tree trunks to get wagons through muddy stretches. The dirt road was eventually paved, and by the 1930s, a nine-foot wide concrete road beginning at Boones Landing was laid from Wilsonville to Oregon City.

Boones Ferry Road was the main street for the original trading center of Boones Ferry and Boones Landing. As the town grew and was officially named, Wilsonville, in 1880, the number of buildings also grew. Some of these buildings of the original settlement are still standing.

In more recent years, Wilsonville has continued to grow. Most of the recent development in Wilsonville occurred after the Boone Bridge and Interstate 5 construction in the late 1950s and the incorporation of the City of Wilsonville in 1969. At this time, the city's population was 1,000. In 1990, the population totaled 7,106. The most recent population statistic in 2006 marks the population at approximately 16,885. The city continues to grow and the street continues to change.



Development Projects

Currently, several development projects along Boones Ferry Road are planned. Fred Meyers, Inc. owns a commercially zoned property between Wilsonville Road and Bailey Street. The store has plans for mixed-use development with its store as the retail anchor. When the development occurs, Boones Ferry Road will be widened from Wilsonville Road to Bailey Street.

Between Bailey Street and Fifth Street, a mixed-use housing development and a building remodel are permitted for construction. An extension of Bailey Street also is planned. This street will connect Bailey Street to Fifth Street, providing an additional exit and circulation for the Old Town district.

At Boones Ferry Park, playground improvements are planned. A pedestrian bridge across the Willamette River is included in the future vision. The bridge will serve as a route for bicycles and emergency vehicles. A trail along the north bank of the Willamette River also is part of future plans.

[Note: The inclusion of the bicycle/pedestrian bridge over the Willamette River and the east side connector were deleted from this Plan by Resolution No. 2324, adopted 9/19/11.]

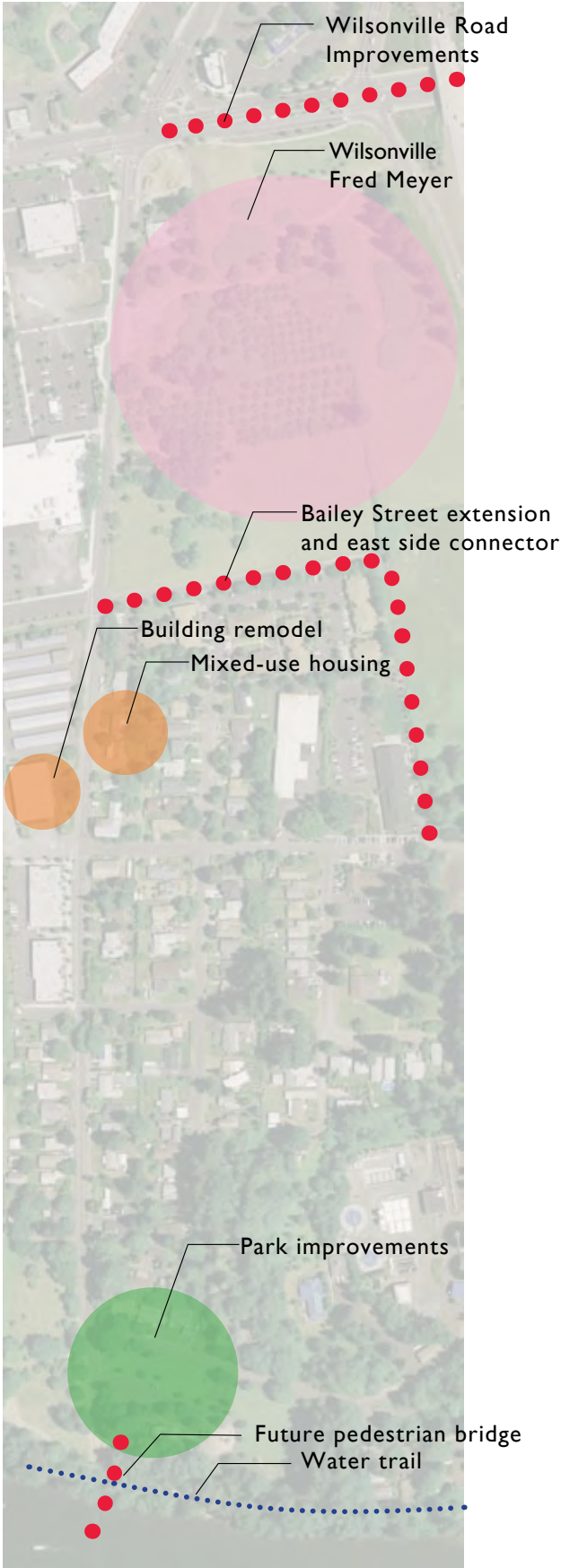


Figure 2: Diagram of Planned Future Development

### General Development Concept

The general development concept incorporates the idea that street use, character, and traffic volume change in intensity from the north to the south of Boones Ferry Road, see Figure 3. Because of these variations through Old Town, scale, formality, and materials should change to respond the various conditions.

#### General Concept Guidelines:

- Recognize that street use and character varies from Wilsonville Road to the River
- Provide for pedestrian safety with continuous sidewalks from Wilsonville Road to the river
- Terminate the road in the park with a feature to commemorate the historic ferry crossing
- Provide bikes lanes to Fourth Street on both sides of the road
- Allow vehicles and bikes to share the road between Fourth Street and the park
- Provide lane widths and street detailing to reinforce a more casual feel closer to the river
- Capitalize on river views
- Provide curb extensions to emphasize crosswalks at intersections
- Protect mature trees
- Provide opportunities for plantings along the road
- Derive character from simple and utilitarian historic details. Employ streetscape elements that are specific to Wilsonville, Boones Ferry, and Old Town as much as possible

### Recommendations

Specific recommendations are made for each of the four areas of Boones Ferry Road. The recommendations are designed to maintain the distinct qualities of each section. Recommendations are discussed in the following sections.

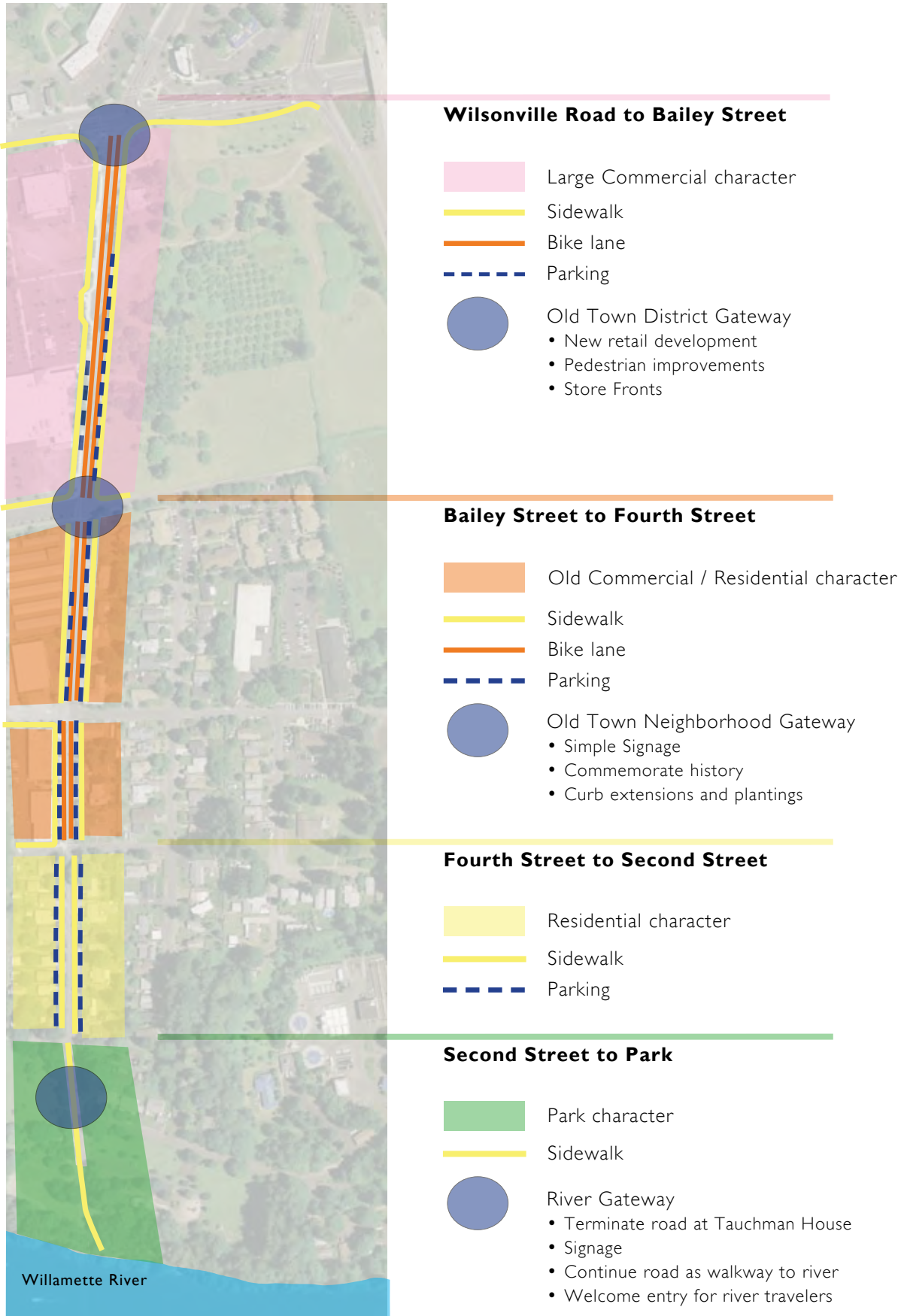


Figure 3: Diagram of Proposed Concept



LARGE COMMERCIAL

Wilsonville Road to Bailey Road

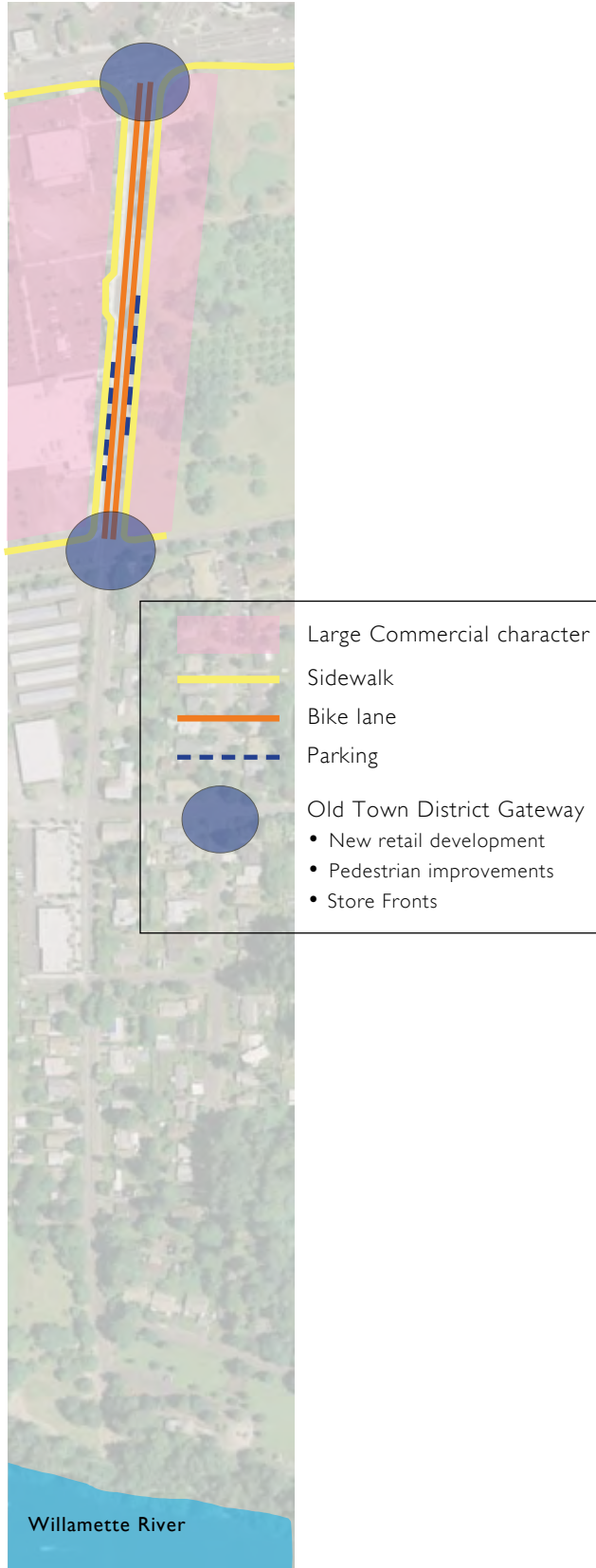


Figure 4: Concept Plan

This area is characterized by existing and planned large retail development. The west side of the road is fully developed with walks, plantings, and street furnishings.

The east side of the road is primarily undeveloped at this time. A Fred Meyer shopping center and potential commercial storefront development along Boones Ferry Road are in the planning stages.

Future development on the road’s east side includes street front-commercial, the existing bank, and potentially the historic church. New street and right-of-way development should be specific to these uses.

Primary considerations include:

- Respond to planned large-scale commercial development
- Walks, lighting, and plantings should respond to the scale of the commercial development



Existing Character of Large trees near Albertsons Grocery Store



Existing Road Includes Sidewalk, Parking, and Single Bike Lane

# RECOMMENDATIONS



Historic Four Corners Intersection at Wilsonville Road and Boones Ferry Road. Location for Old Town District Gateway



Stormwater Planters and Street-Side Parking Example

## GENERAL RECOMMENDATIONS

- Define the Old Town district with a gateway feature. A sign, marker, or plantings could serve as this gateway
- Provide curb extensions where possible to ease pedestrian crossing across Boones Ferry Road
- Install concrete sidewalks with curbs
- Provide parking at storefront commercial areas
- Provide bike lanes on both sides of the road
- Install stormwater planters in landscaped areas
- Plant street trees. Select tree species that mature to a large size and thrive in the dry summer and the wet winter climate. Spacing should be a maximum of 40 feet on center for large canopy trees and less for smaller trees
- Install streetlights
- Provide landscaped grounds at the historic church site

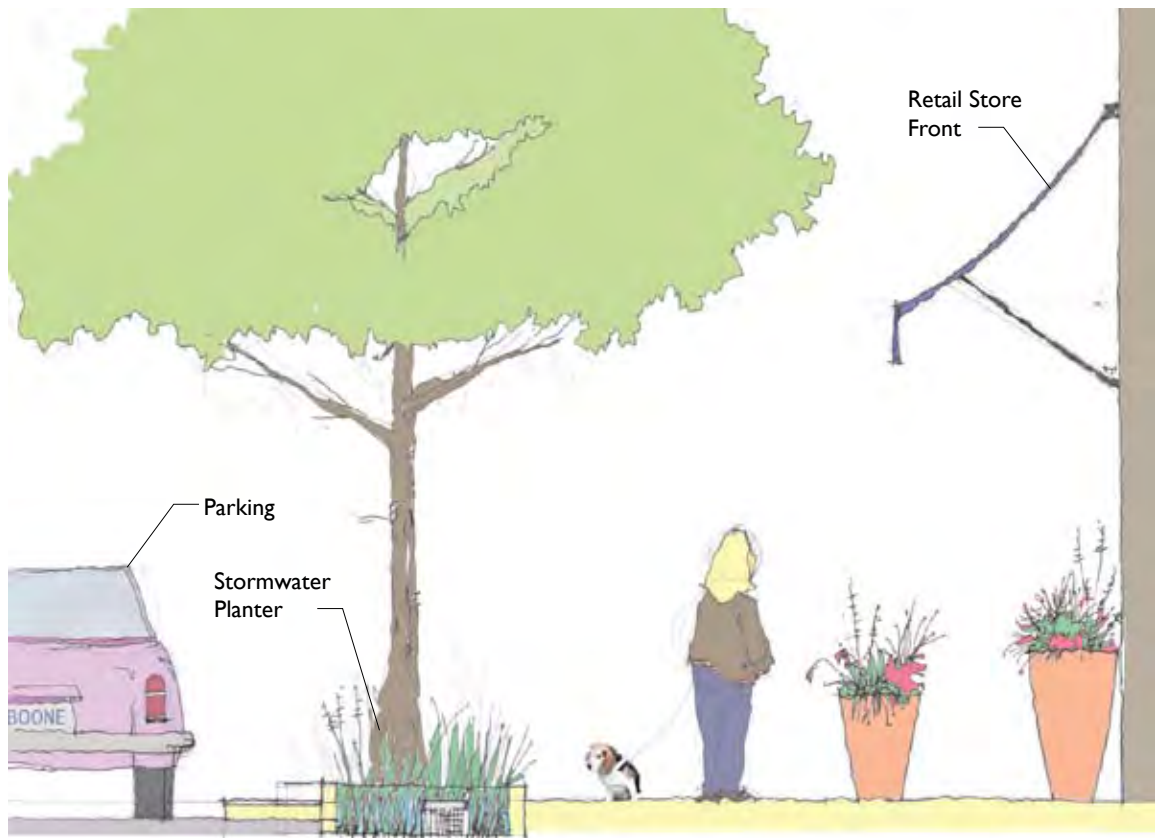


Figure 5: Parking, Stormwater Planter, and Sidewalk at Retail Store Front

# RECOMMENDATIONS

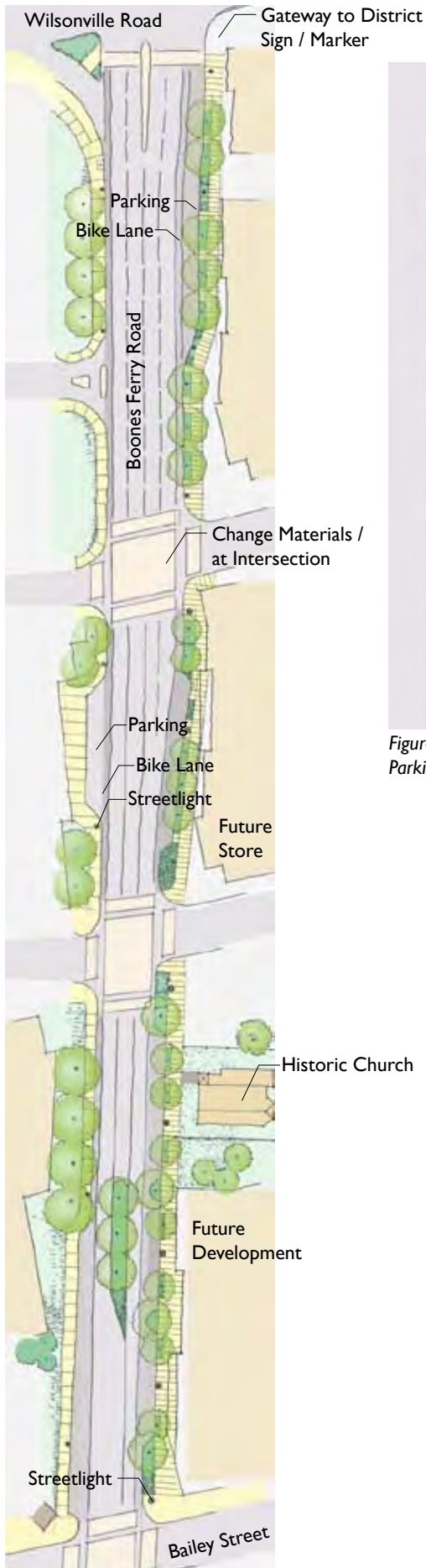


Figure 6: Wilsonville Road to Bailey Street Proposed Plan  
Appendix C  
Old Town Neighborhood Plan

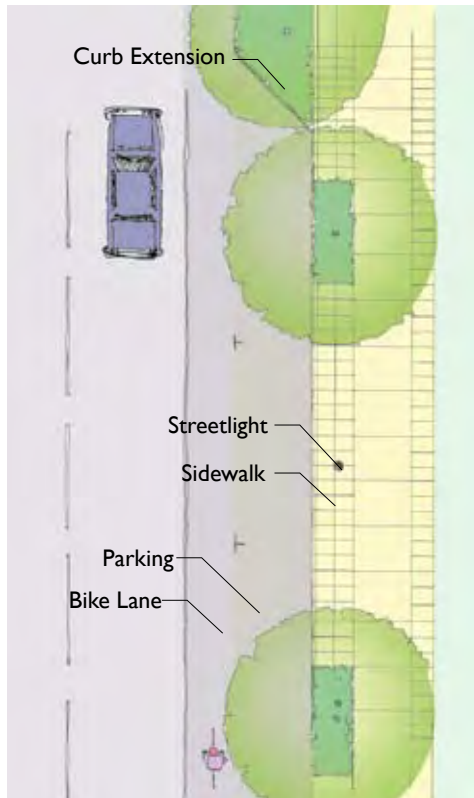


Figure 7: Enlarged Plan with Street Elements: Bike Lane, Parking, and Sidewalk

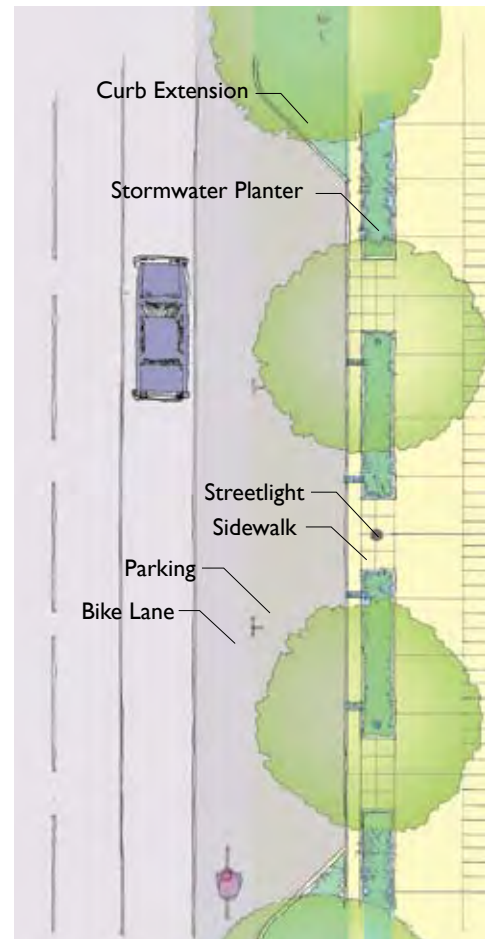


Figure 8: Enlarged Plan with Street Elements and Stormwater Planters





OLD COMMERCIAL / RESIDENTIAL

Bailey Street to Fourth Street

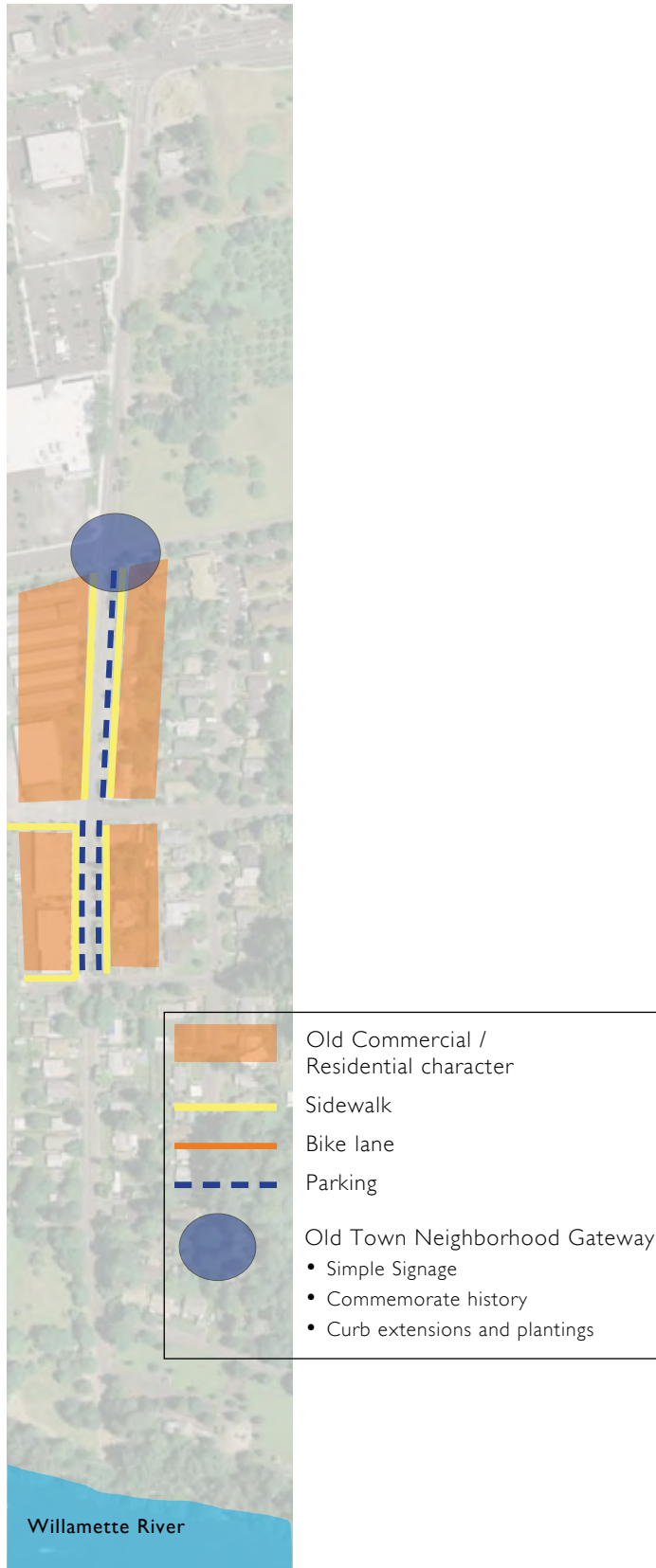


Figure 9: Concept Plan

This area of Boones Ferry Road has the greatest variety of structures and uses. It has the newest and oldest structures in the neighborhood, including an assortment of apartments, old commercial, new commercial, and mixed residential.

Right-of-way improvements also vary. In some locations there are formal curbs, walks, parking, plantings, and lighting. While in other areas, the street lacks curbs, walks, or paved parking.

Primary considerations include:

- Address the diversity of neighborhood use
- Maintain the casual character of the neighborhood and the road
- Provide pedestrian and bicycle connections through the neighborhood
- Existing driveways will need to be incorporated into the improvements
- Maintain on-street parking



Existing Commercial development at Fourth Street



Existing Commercial Retail at Fifth Street

## RECOMMENDATIONS



Existing



Figure 10: Proposed Gateway Old Town Sign with Bicycle Lane at Bailey Street Intersection



Existing



Figure 11: Proposed Stormwater Planter and Boardwalk at Historic

## RECOMMENDATIONS

- Define Old Town with a gateway feature that identifies the entry to the neighborhood. A simple sign, plantings, or sculpture could serve as this gateway
- Provide curb extensions at Second, Third and Fourth Streets to scale down the road, make pedestrian travel easier and provide areas for planting and stormwater treatment
- Install curbs and sidewalks at all new developments
- Provide bike lanes on both sides of the road
- Provide stormwater planters along the road to assist with drainage and provide aesthetic appeal Provide on-street parking in front of all buildings
- Maintain all driveway access into private property
- Install historic lighting on both sides of the street
- Plant additional street trees as properties re-develop
- Make plantings contribute and be distinctive
- Maintain on street parking throughout the district
- Treat historic buildings as special conditions. Provisions should be made for improvements appropriate to their use and character. Boardwalks, casual stormwater planters, and furnishings should be designed to respect the historic buildings. Street trees should not interfere with the architecture

# RECOMMENDATIONS

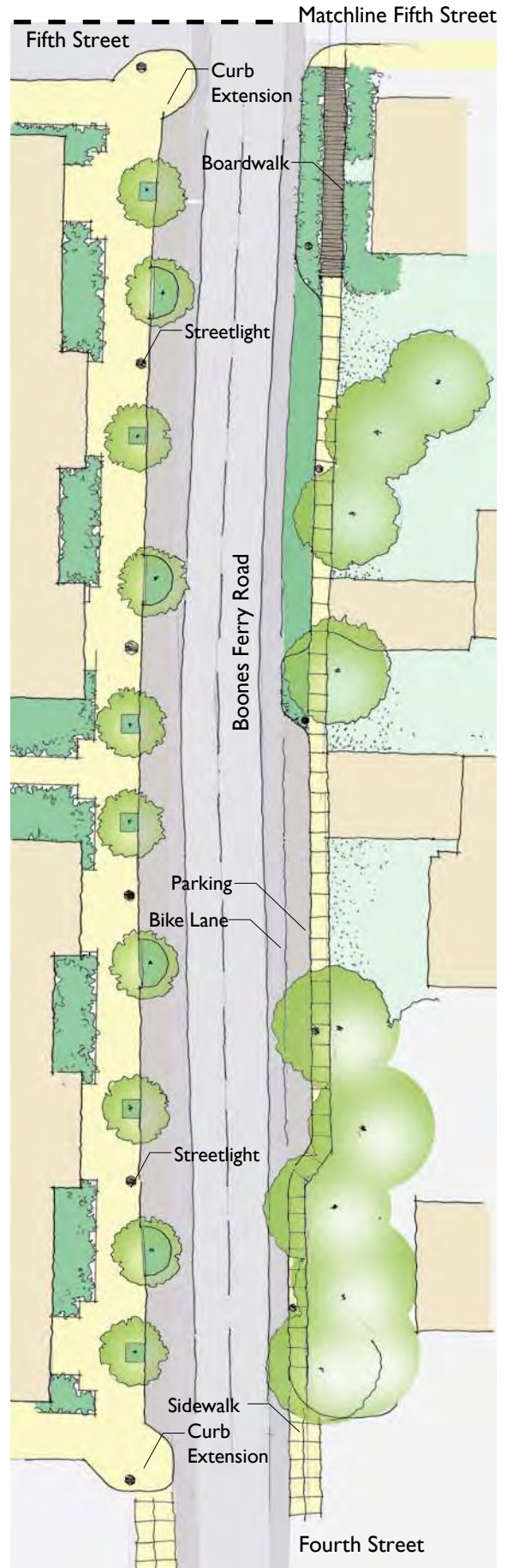


Figure 12: Bailey Road to Fourth Street Proposed Plan  
Page 19



RESIDENTIAL

Fourth Street to Second Street

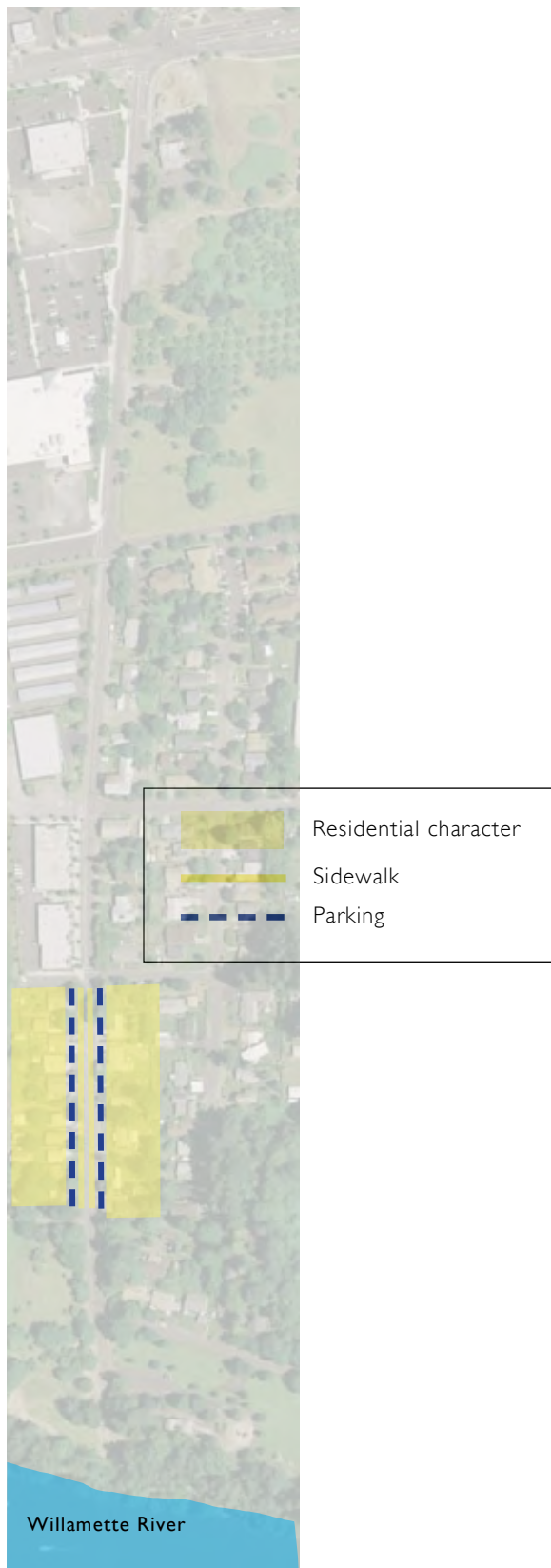


Figure 13: Concept Plan

Fourth Street to Second Street

This area is characterized by older residential development. Existing streets are built to casual rural standards without curbs, sidewalks and formal parking areas. In this area, large trees overhang homes and the street, provide shade, and help to reinforce the historic character. Picket fences, front yard gardens, and street side mailboxes all contribute to the casual feel of the neighborhood.

In some areas, private plantings and development encroach onto the right of way. Traffic primarily serves the park, the homes, and the sewage treatment plant.

Primary considerations include:

- Address the diversity of the neighborhood
- Maintain the casual character of the neighborhood and the road
- Provide safer pedestrian connections through the neighborhood



Existing Single Bike Lane and Large Trees



Existing Residential Character Includes Curbless Road and Picket Fence

# RECOMMENDATIONS



Residence in 1900s Wilsonville Includes Picket Fence

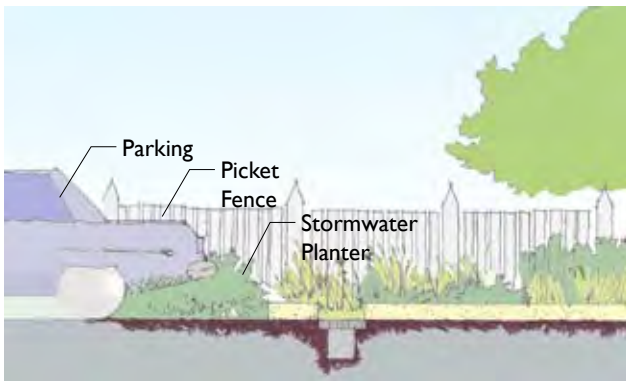


Figure 14: Parking, Stormwater Planter, and Residential Picket Fence

## RECOMMENDATIONS

- Maintain the casual curbless street
- Provide an on-grade concrete sidewalk for pedestrians located between the road and parking areas
- Terminate bicycle lanes at Fourth Street. Bikes and vehicles share the road from Fourth Street to the river
- Continue stormwater swales in the street right-of-way between parking areas and the proposed sidewalk
- Define parking spaces with pervious pavement or gravel
- Encourage elements that contribute to the casual residential character of the street. These could include a variety of picket fences to define private property, gardens to enhance the residential scale of the area, and planting of large shade trees that will provide a canopy over the street
- Protect all large old trees by minimizing root zone impacts on any construction

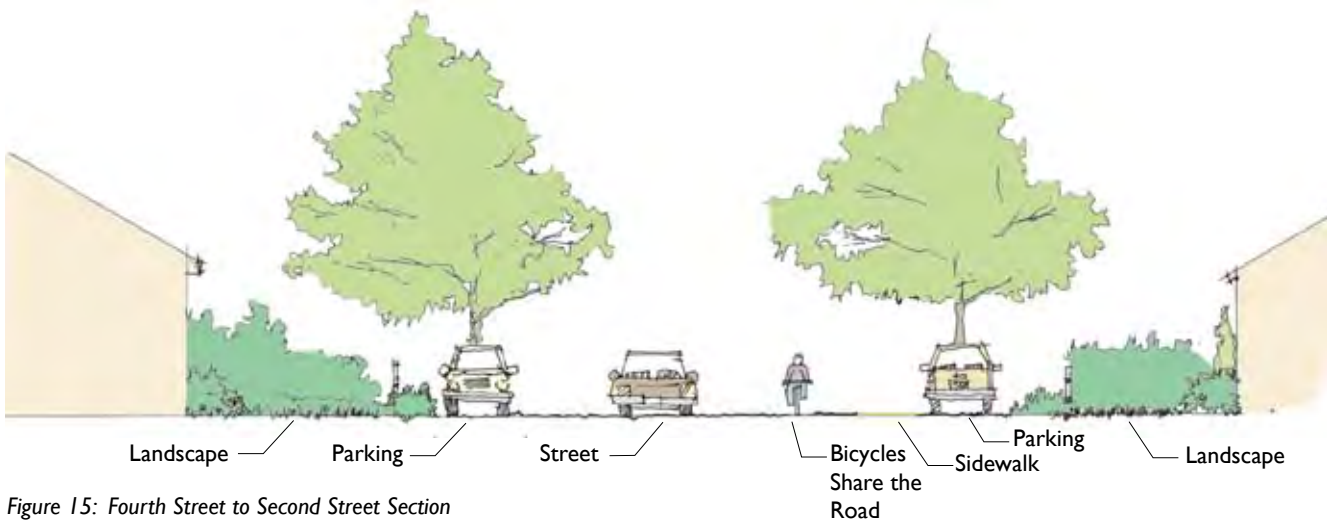


Figure 15: Fourth Street to Second Street Section

# RECOMMENDATIONS

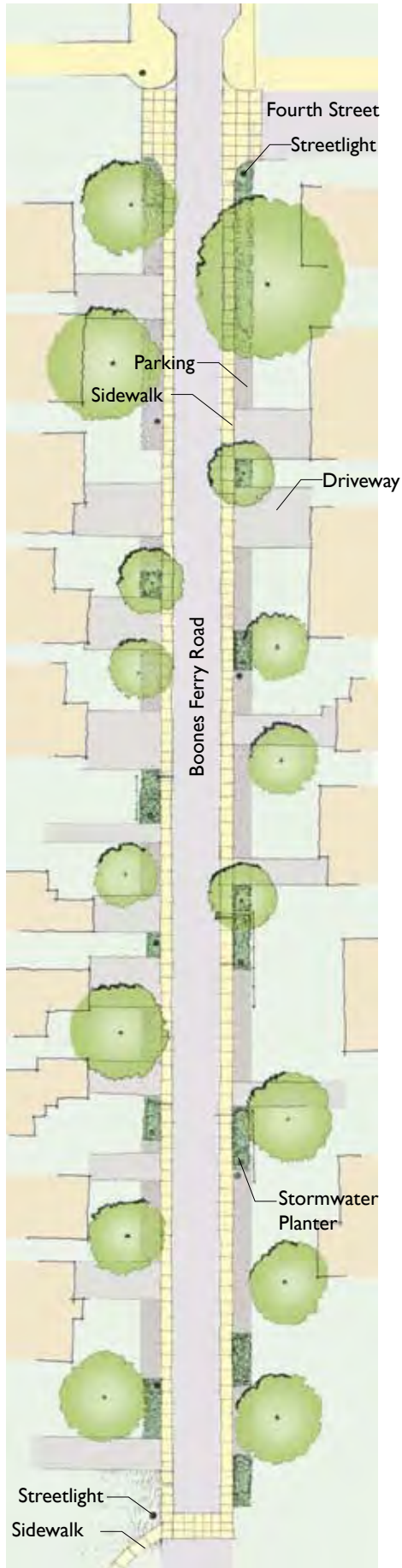


Figure 16: Fourth Street to River Proposed Plan

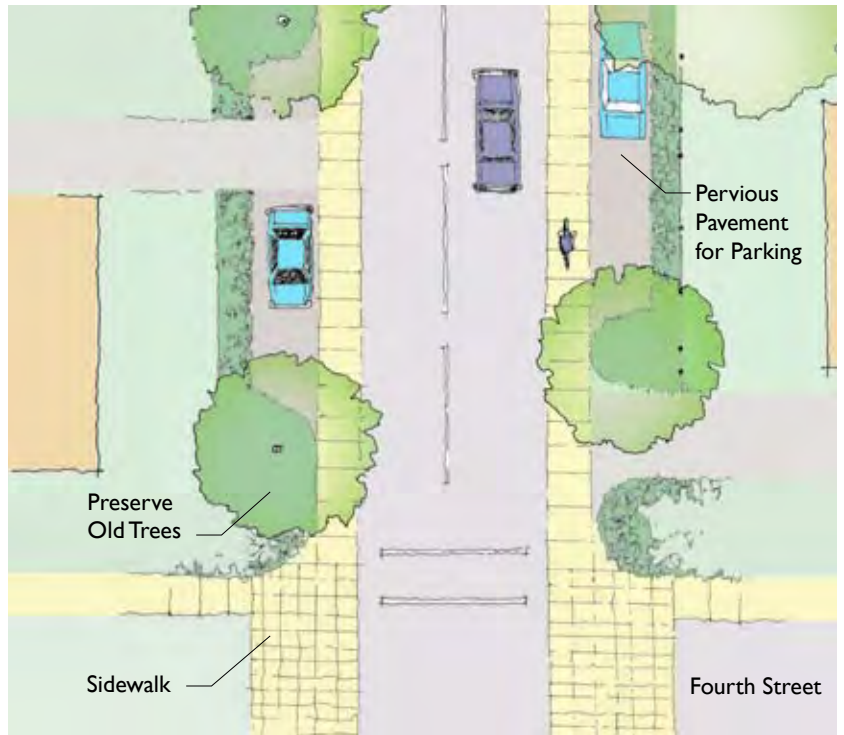


Figure 17: Enlarged Plan with On-Grade Sidewalks, No Bike Lanes and Casual Parking



Figure 18: On-Grade Sidewalk and Stormwater Planter



Figure 19: Proposed Curb Extension at Fourth Street



Existing





PARK

Second Street to Park



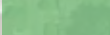
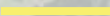

	Park character
	Sidewalk
	River Gateway
	<ul style="list-style-type: none"> <li>• Terminate road at Tauchman House</li> <li>• Signage</li> <li>• Continue road as walk to river</li> <li>• Welcome entry for river travelers</li> </ul>

Figure 20: Concept Plan

This section of Boones Ferry Road includes the southern terminus of the street at the park, the historic Tauchman House, and the walkway to the river. Glimpses of the river and railroad bridge can be seen from the road.

Existing right-of-way improvements include the road and casual parking along the road. Because Boones Ferry Road is a dead end, little traffic exists at this end of the street.

Primary considerations include:

- Maintain the casual character of the park and road
- Respect and capitalize on the valuable historic location



Existing Character of Trail to Willamette River at Boones Ferry Park



Existing Road Includes Curbless Street, Bike Lane and Remnant Orchard

# RECOMMENDATIONS



Historic Tauchman Farm and House in 1909



Figure 21: Proposed River Gateway at End of Boones Ferry Road

## RECOMMENDATIONS

- Provide sidewalks from the neighborhood to the Tauchman House. Continue a river walkway to the historic river crossing. (Grades are too steep to be considered accessible by the Americans with Disabilities Act; however, an improved walk would still provide safe pedestrian access to the riverbank.)
- Maintain curbless road
- Provide a lighted walk to the Tauchman House. Streetlights should have features to minimize bright lights
- Plant trees in the park, in parking areas and along the sidewalks. Trees shown in concept plans are spaced to mimic the historic orchards
- Explore opportunities for stormwater treatment in the park
- Re-grade the existing ditch on the east side of the road to minimize erosion, improve views into the park, and simplify mowing
- Re-create the historic fencing / guardrail along the road to the ferry crossing
- Commemorate the ferry crossing with a small plaza, signs, and fencing

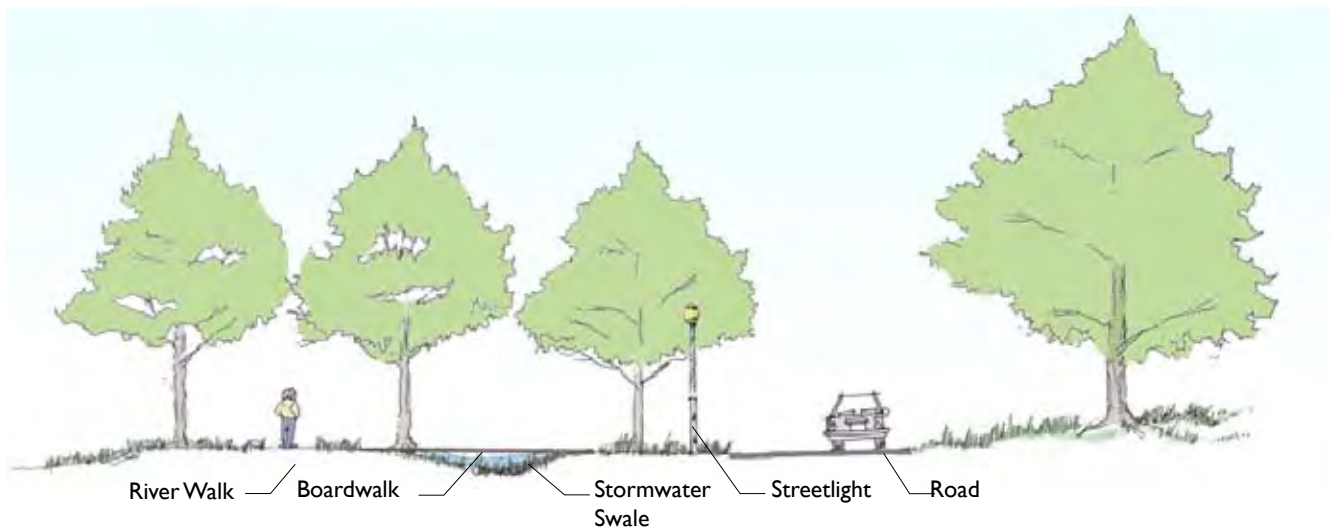


Figure 22: Park to Willamette River Section



Figure 23: Second Street to Park Proposed Plan

## RECOMMENDATIONS

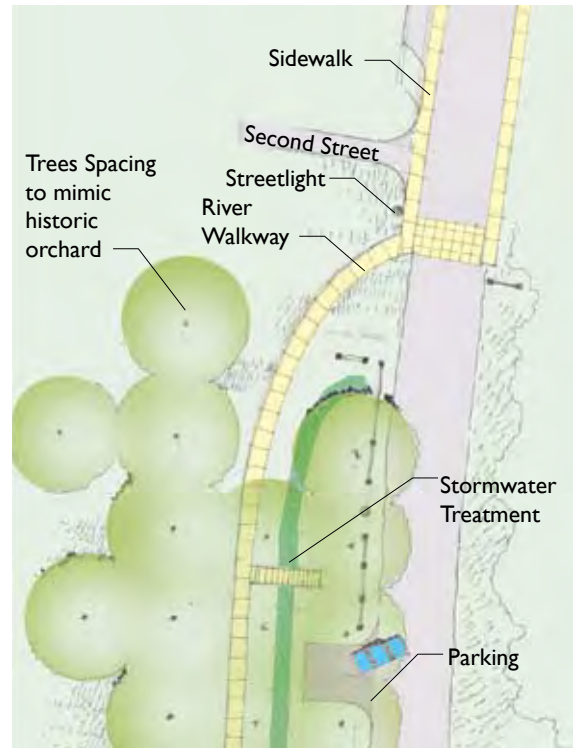


Figure 24: Plan Enlargement at Second Street and Boones Ferry Road



Boones Ferry and Historic Fence Railing, 1950



Figure 25: Historic Rail Fence Example along River Walkway

# RECOMMENDATIONS MATRIX

## Wilsonville Road to Bailey Street

Feature	Existing	Proposed	Materials /Comments
Road Lanes	12 feet	Lanes / Widths will be developed as part of the development plan	
Sidewalks	0-8 Feet	6-Foot Walk 4-Foot Furnishing Zone	Widths shown are minimums. Additional width at storefronts for tables, benches, commercial displays, and plantings may be possible
Stripes	Formal Striping Plan	Formal Striping Plan as part of road development	
Bike Lanes	5-Foot Lane on the west side of the street	5-Foot Lanes on Both Sides of the Street	
Parking	No Formal	On-Street Parking at storefront development	On-street parking could utilize pervious pavement to minimize stormwater impacts and as a demonstration for the city
Curbs	None	Concrete Curbs	Curbs may have breaks to allow stormwater to flow into stormwater planters
Lights	Existing Lights on west side of the street	Match West Side of Street	
Street Trees	Ornamental Oak	Plant Large Deciduous Trees that compliment the trees on the west side of the road	Plant trees that will thrive in dry summer conditions. Provide adequate tree well for trees, minimum 4-6 feet
Benches / Furnishings			Pots / Benches
Green Streets / Drainage	None	Possible to Continue Green Stormwater Planters along the street	Continuation of stormwater planters can help to provide a continued theme and connection to the river

**Bailey Street to Fourth Street**

<b>Feature</b>	<b>Existing</b>	<b>Proposed</b>	<b>Materials /Comments</b>
Road Lanes	12 feet	12 Feet	
Sidewalks	0-8 Feet	8 Feet	Walks can be larger at commercial areas and smaller in front of residential areas, minimum 6-feet at residential, commercial areas can be as wide as 12-feet, depending on use
Stripes	Stripe Travel Lanes	Stripe Travel Lanes	
Bike Lanes	One 5-Foot Lane	5-Foot Lanes on both sides of the street	
Parking	On-Street Parking	On-Street Parking	Formalize on-street parking when residential uses convert to retail
Curbs	Varies	Curbs	
Lights	Varies	Historic Light Fixtures	
Street Trees	Mix of Trees	Medium Scale Deciduous Trees	Maintain and promote larger trees on residential properties and in larger tree well opportunities in right-of-way
Benches / Furnishings		Provide Benches	Simple utilitarian benches
Green Streets / Drainage			Possible retrofit of existing planters

# RECOMMENDATIONS MATRIX

## Fourth Street to Second Street

Feature	Existing	Proposed	Material / Comments
Road Lanes	10-11 feet	11 Feet	Need to confirm specific lane widths when the area is surveyed. Widths may need to be altered to preserve trees
Sidewalks	None	4 Feet at Edge of Road	On-grade scored concrete
Stripes	None	None	
Bike Lanes	One 6 Foot Lane	Bikes to Use Vehicle Lanes	Provide signs to indicate lanes have ended
Parking	Parallel Parking On-Street	Parallel Parking On Street	Pervious paving or gravel. Pervious Paving may work better to keep rock off sidewalk
Curbs	None	None	Curbs may be incorporated into stormwater planters, if implemented for auto and pedestrian safety
Lights	Cobra Lights	Historic Fixture with Cut Off Feature	Employ existing historic fixture
Street Trees	Variety of Species. Many in residential landscapes and not in right-of-way	Promote Large Native Trees	Maintain all mature trees
Benches / Furnishings	None	None	Encourage fencing and other detailing adjacent to the right-of-way to compliment historic character
Green Streets / Drainage	None	Provide Opportunities for Stormwater Treatment in Parking Zone	

**Second Street to Park**

<b>Feature</b>	<b>Existing</b>	<b>Proposed</b>	<b>Material / Comments</b>
Road Lanes	10 feet	10 Feet	Asphalt
River Walkway	10 feet	10 Feet	Concrete
Sidewalks	None	6 Feet	Concrete
Stripes	None	None	
Bike Lanes	None, In Road	None, Bikes Share Road	Bicycles Share Road with Cars
Parking	Parallel Parking	Option 1: Parallel Option 2: Perpendicular	Parallel Parking
Curbs	None	None	
Lights	None	Provide Lights with cut off feature	Match existing features
Street Trees	Mix of Volunteer Trees and Mature Native Trees	Native Trees and Historic Trees to mimic historic orchard	
Benches / Furnishings	None	Benches along walkway at river overlook. Fencing / Rail Fence	Wood benches. Research historic railing and restore along walkway
Green Streets / Drainage	Ditches and Street Drainage	Formalize the Drainage to minimize erosion and treat stormwater	Provide stormwater planters to accommodate runoff





**APPENDICES**

APPENDIX I: Power Point Presentation in attached CD format

APPENDIX II: Survey/Public Comments



# Appendix C:

## Boones Ferry Road Streetscape Project

By

Nevue Ngan Associates

**Appendix I:**  
*(of February 2008 Technical Memorandum)*  
**Power Point Presentation**



## Boones Ferry Road



## Agenda

### **Boone's Ferry Road Streetscape Plan Public Meeting #2**

---

- 6:00 - 6:15    Review Drawings**
- 6:15            Introduction - Sandi Young**
- 6:20 - 6:30    Presentation - Bo Nevue**
- 6:30 - 7:30    Group Discussions**
- 7:30 - 8:00    Group Presentations**
- 8:00            Old Town Plan Presentation**

**Goals of the Meeting**  
Review input from Meeting 1  
Review preliminary concepts  
Comment and give direction on preliminary ideas

## Project Goals

### Goals

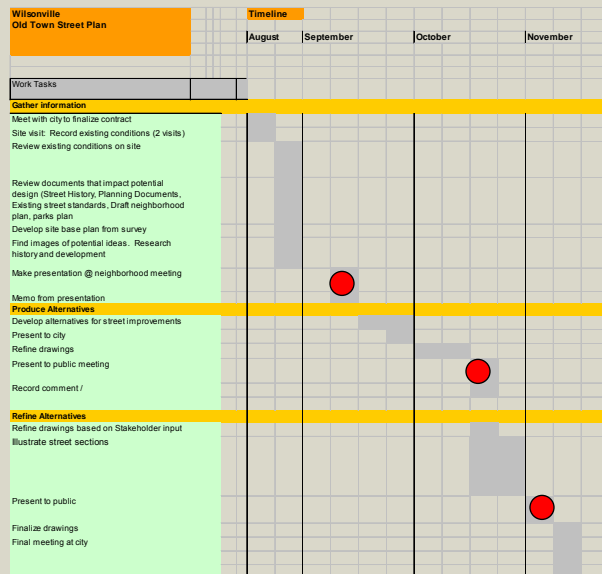
Develop street standards for Boones Ferry Road from Wilsonville Road to Boones Ferry Park

### Review

- Lanes
- Walks
- Plantings
- Furnishings (benches, bike racks, etc)
- Lighting

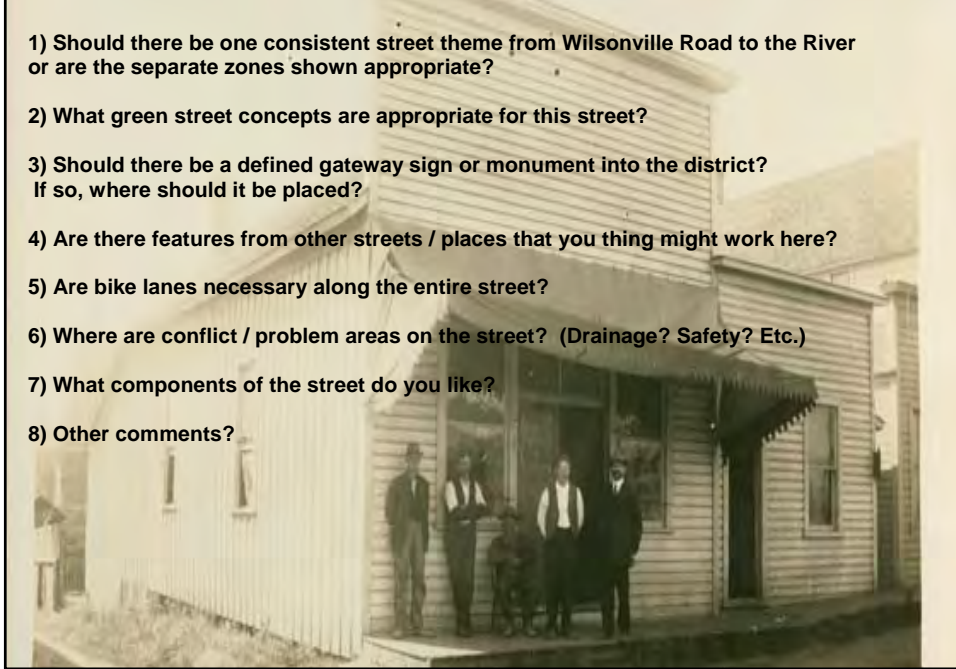
- Develop Standards that are appropriate for various areas
- Respect and capitalize on the roads and district history
- Make development compatible with city engineering standards
- Investigate concepts for low-tech or green solutions for stormwater
- Support businesses and respect homeowners
- Develop standards that are consistent with other city planning efforts

## Timeline



## Previous Meeting Discussion Questions

- 1) Should there be one consistent street theme from Wilsonville Road to the River or are the separate zones shown appropriate?
- 2) What green street concepts are appropriate for this street?
- 3) Should there be a defined gateway sign or monument into the district? If so, where should it be placed?
- 4) Are there features from other streets / places that you think might work here?
- 5) Are bike lanes necessary along the entire street?
- 6) Where are conflict / problem areas on the street? (Drainage? Safety? Etc.)
- 7) What components of the street do you like?
- 8) Other comments?



## Previous Meeting Comments – Group 1

- Question #1
- **Lighting:** Reduce the scale towards the river
  - **Sidewalks:** Reduce the scale towards the river
  - **Drainage:** Open to green ideas
  - **Bike Paths:** On street south of 4<sup>th</sup> Street
  - **Bump Outs**( curb extensions) only North of 4<sup>th</sup> Street
  - **Parking:** Needed on the street all the way to the river
  - **Landscape:** More casual / naturalistic south of 4<sup>th</sup> Street
- Question #2
- **Yes.** Some concepts more naturalistic south of 4<sup>th</sup> Street, less urban, more wild.
  - **Green drainage strip** south of 4<sup>th</sup> Street in front of houses
  - **More urban** North of 4<sup>th</sup> Street to achieve traffic calming functions
  - **Swales** possible in front of houses south of 4<sup>th</sup> Street
- Question #3
- **If so** where should it be placed
  - **Yes**
  - **Various opinions.** Probably between Bailey and 5<sup>th</sup> Streets
- Question #4
- **West Linn @ 10<sup>th</sup> Street**
  - **Old town Lake Oswego**
- Question #5
- **Bike lanes** both sides to 4<sup>th</sup> Street
  - **Shared travel lanes** from 4<sup>th</sup> Street to the river
- Question #6
- **At 5<sup>th</sup> Street** traffic and speed
  - **At 4<sup>th</sup> Street** U-turns, speed,
- Question #7
- No Answer

## Previous Meeting Comments – Group 2

Question #1

**No. There should not be a consistent theme.**

Question #2

- **On street for some parts of Boones Ferry**
- **Off street for other parts of the Street**
- **Green street at North East side of Boones Ferry at 4<sup>th</sup> Street**
- **No sidewalk E side of Boones Ferry between 4<sup>th</sup> and 5<sup>th</sup> Street**

Question #3

- **Signage for old town at Bailey Street**
- Old town flavor ... logo from historical society

Question #4

- **Willamette area of West Inn**
- **First Addition are of Lake Oswego**

Question #5

**Yes**

Question #6

- **Drainage of Boones Ferry on the East side between 4<sup>th</sup> and 5<sup>th</sup> Street**
- **Conflicts between 4<sup>th</sup> and 5<sup>th</sup> Street with poor business traffic flow**
- **Elevations of curbs and sidewalks need to be determined for future homes to be constructed between bailey and 5<sup>th</sup> Street**

Question #7

- **Trees**
- **Dead End**

Question #8

- **Can the church be moved?**
- **Don't mess with historic buildings**
- **If sidewalk is necessary make them similar to those in front of St. Cyris Church**
- **Preference to no sidewalks or curbing in residential areas**
- **Closer to the river there could be the same basic plane but different surfaces that delineate where bikes/ pedestrians and cars travel.**
- **Soft vintage lighting**
- **More rural feel towards the river**
- **Curb extensions at 5<sup>th</sup> and 4<sup>th</sup> for green street elements**

## Previous Meeting Comments – Group 3

Question #1

- **There should be some unifying theme along the street, but realize that there are definite zones along the street corridor. Potentially lighting, green street elements, artwork, or historical signage can be the consistent theme.**

Question #2

- **Alternating stormwater curb extensions could provide a sinuous travel pattern along the street.**
- **Roundabouts were mentioned.**
- **The general addition and preservation of street trees**

Question #3

- **Liked the "Welcome to Wilsonville" type of gateway sign**
- **Could be a primary gateway at Wilsonville Road intersection and a secondary gateway at Bailey Road**
- **Perhaps gateway signage could be incorporated in roundabouts**

Question #4

**No response**

Question #5

- **Yes, would like to see the existing bike path remain along the entire street**
- **Would be open to having a shared bike path/pedestrian path condition**

Question #6

- **Parking in between 4<sup>th</sup> and 5<sup>th</sup> Streets is already congested**
- **People constantly speed down the street (especially the SMART Bus)**
- **Cars/Trucks make U-turns along Boones Ferry especially at 4<sup>th</sup> Avenue**
- **Considerable ponding of water occurs along the east shoulder of the road between 4<sup>th</sup> Street and Tauchman Road.**

Question #7

- **Like the historical looking lighting near Albertsons**
- **Like the park space at the end of Boones Ferry**
- **Like the existing bike path (mostly used for joggers and pedestrians)**

Question #8

- **General concern about new development occurring and having building massing over 2-stories**
- **Liked the idea of having artwork along the street, potentially mixed in with green street elements**
- **Generally like the idea of a bike bridge connection across the Willamette River**
- **Would like to see the riverfront at the terminus of Boones Ferry Road enhanced.**
- **-If more lighting is introduced, we are concerned about light pollution.**



Willamette



Willamette



Willamette



Willamette





Willamette



Willamette





Sherwood



Sherwood

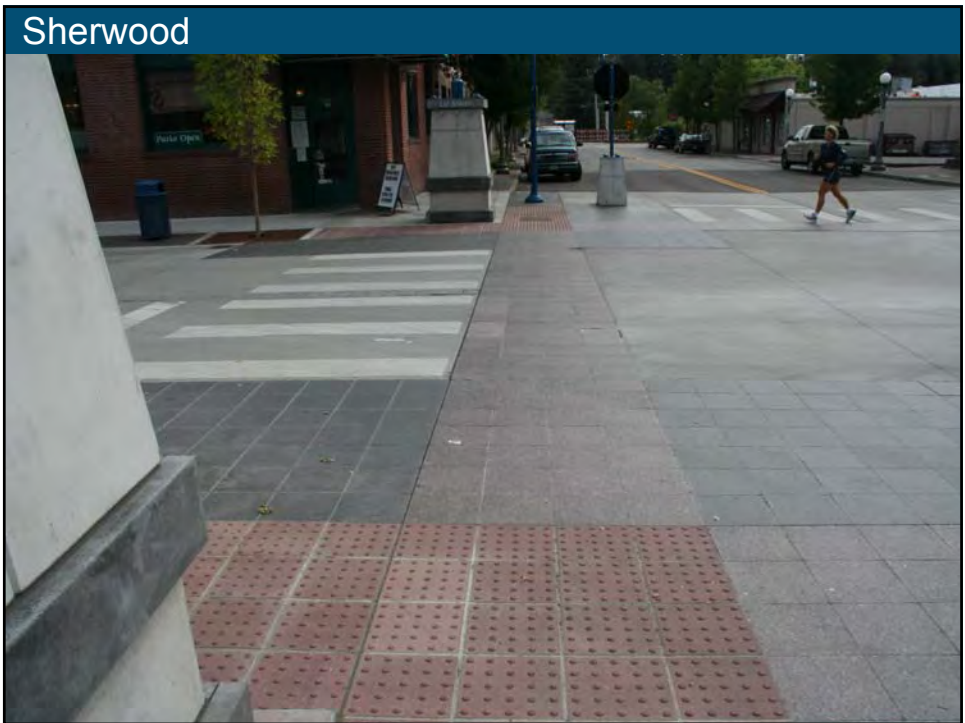
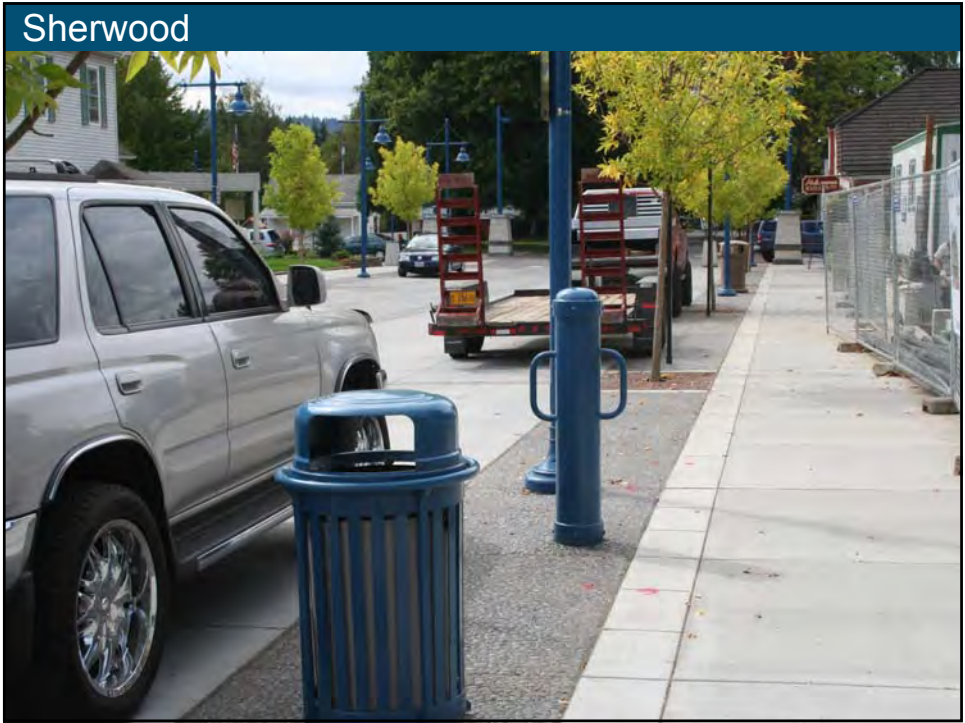


Sherwood



Sherwood







Sherwood



Sherwood



## Sherwood

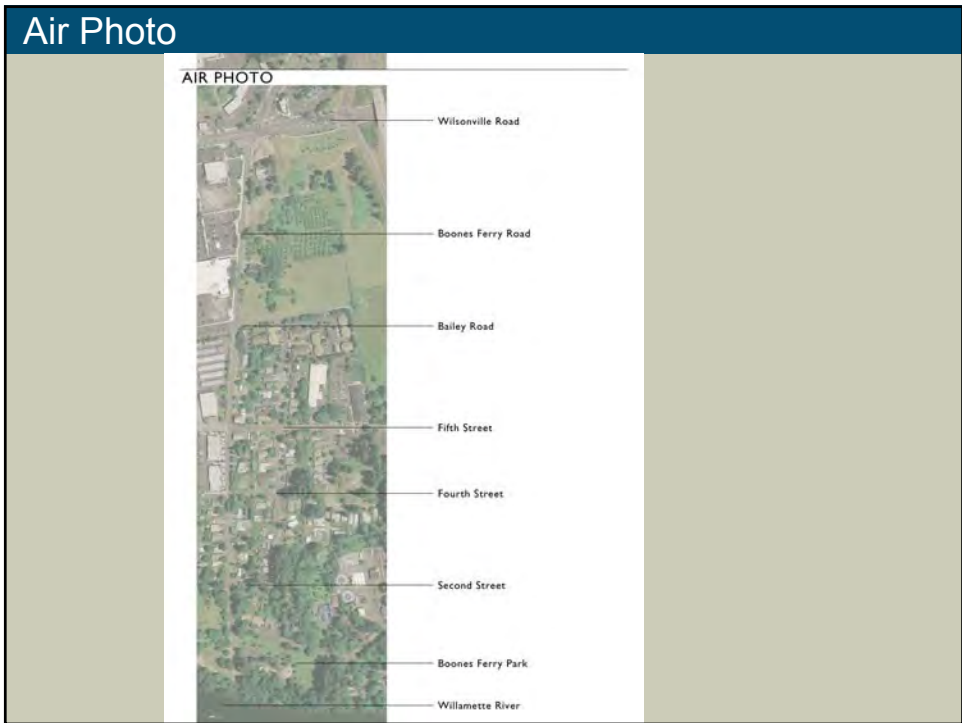


## Existing Boones Ferry Road





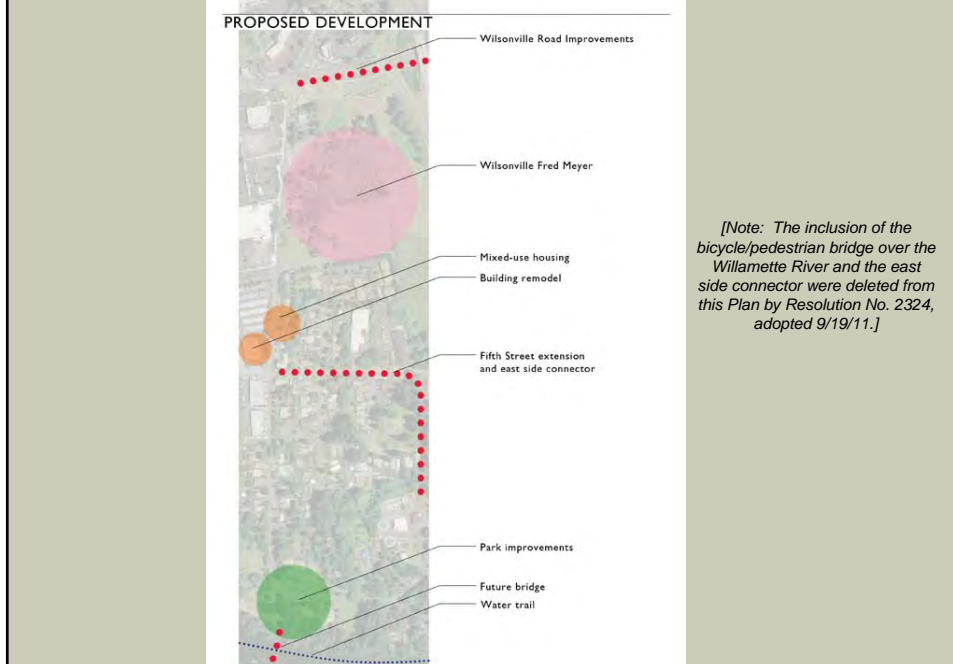




## Existing Conditions



## Proposed Development



## General Ideas

- Recognize that street varies from Wilsonville Road to the River
- Provide continuous sidewalks from Wilsonville Road to the river
- Terminate the road with some feature at the park to commemorate the crossing
- Bikes lanes on both sides of the road or not on either side of the road
- Change scale. Make street seem smaller, slower and more rural towards the river.
- Maintain the casual character of the district (not a too self conscious design)
- Capitalize on view to river and historic crossing
- Curb extensions to emphasize crosswalks where there is side street parking
- Protect the mature trees Drainage
  
- Change the character along the street of typical elements.
  - Pavement
  - Lights
  - Trees
- Derive character from historic details. Simple, utilitarian and hopefully authentic might work best. Use elements that are specific to Wilsonville, Boones Ferry, and Old Town District as much as possible

## Historic Features



Four Corners

### Orchard and Historic Concrete Road





# Appendix C:

## Boones Ferry Road Streetscape Project

By

Nevue Ngan Associates

**Appendix II:**  
*(of February 2008 Technical Memorandum)*  
**Survey/Public Comments**



**MEMORANDUM**

**TO:** Sandi Young  
**FROM:** Bo Nevue  
**DATE:** 10-31-07  
**SUBJECT:** Meeting Notes

A public meeting was held on October 29th, to review design concepts related to Boone's Ferry Road through Old Town Wilsonville. Approximately 30 people attended the meeting which was held at St. Cyril Catholic Church. Comments are as follows:

<b>Boones Ferry Road Streetscape, Meeting # 2 – Alternatives</b>			
<b>Discussion Questions</b>	<b>Group 1</b>	<b>Group 2</b>	<b>Group 3</b>
Should Old Town Wilsonville be more like Sherwood or Willamette?	This is comparing apples to oranges. Closer to Willamette. Wilsonville is different. Not as many businesses, a dead-end road, and not as wide of a street. No through traffic. Scale down the street from 4 <sup>th</sup> to Boones ferry / river.	More like Willamette.	Neither. Be distinctive and authentic to Wilsonville.
<b>Wilsonville Road to Bailey.</b> Should the intersection be used to lead people to the historic district?	Leading people down the street is fine but limit the cars through the neighborhood to a dead end road. Want to encourage vehicle traffic only to 4 <sup>th</sup> street. The group has a question whether business can be supported on a dead end street	Gateway should be at Bailey. Identify Old Town at Wilsonville Road	Refer to Old Town overlay with on-street parking.

Discussion Questions	Group 1	Group 2	Group 3
<p><b>Bailey to Fourth</b> Remove bike lane and add planting can scale down street</p> <p>What would be an appropriate gateway into Old Town</p> <p>Other comments on concepts</p>	<p>Yes it is a good idea for neighbors. No it is not a good idea for businesses. Safety issue.</p> <p>Signage or statue may be appropriate. Sophisticated bronze design with information about historic area. Something similar to the sign @ Willamette Falls might be appropriate.</p> <p>What is the cost impact on different choices and how will the projects be funded.</p>	<p>No. Keep the bike lane</p> <p>Signage with an Old Town icon...the ferry with a horse team.</p> <p>None</p>	<p>No. Provide bike lanes on street except at the historic buildings. Provide parking on the east side of the street</p> <p>A monument sign</p> <p>No consensus on this area in the group. Some wanted walks and others did not want walks. Provide parking on the east side of the street</p>
<p><b>Fourth to Second</b> Should the sidewalk be closer to homes or closer to the street</p> <p>Should bike lane and walkway be combined</p>	<p>Sidewalk should be close to the street</p> <p>Don't agree as a group about this</p>	<p>Sidewalks should be close to the street</p> <p>Yes. From 4<sup>th</sup> Street to the river bikes should share the road with traffic</p>	<p>The sidewalk should be close to the street.</p> <p>Combine bikes and vehicles on street and have a separate sidewalk for pedestrians.</p>

Discussion Questions	Group 1	Group 2	Group 3
<p><b>Second to the river</b> Should there be parking on the street in the park</p> <p>Should features be developed to commemorate the history of this site</p>	<p>Yes</p> <p>There is signage there already. Prefer it to be non structural like pictures. Art is subjective and needs neighborhood approval.</p>	<p>Yes</p> <p>Yes</p>	<p>The street should be similar as the 4<sup>th</sup> to 2<sup>nd</sup> section. Keep the “street look” with parallel parking.</p> <p>Yes. Something may be appropriate but first need to determine what it would look like.</p>
<p>Other comments</p>	<p>Park is only 3 acres on the west side of Boones ferry and 7 acres on the east side of the road. . Old Town should not be a destination or regional park. It should remain a neighborhood park. We don’t want to invite the region into our neighborhood.</p> <p>Consider putting a shelter on the other side of the river. It would attract riff-raff. A turn around any where near the residences would impact the homes.</p>	<p>Round about monument and at the end of the road should be located away from Tauchman Road.</p> <p>Provide diagonal parking on the end of the road in the park Move any potential turnaround towards the end of the road.</p> <p>Provide textured surfaces to designate certain areas for sidewalks and bike lanes</p>	<p>Develop the Old Town identity at Bailey. Bike lanes and on street parking except in front of historic buildings.</p> <p>Parking between Wilsonville Road and Bailey should be provided for future development from driveway south.</p> <p>There are approved plans for a redevelopment project at 5<sup>th</sup> Street.</p>



# Appendix C:

## Boones Ferry Road Streetscape Project

By

Nevue Ngan Associates

**PowerPoint supporting Nevue Ngan Associates  
.....Streetscape work**







### Historic Features



**False Facade and Awning**

Roanoke Inn

## Historic Features



Community Hall

## Historic Sign



Picket Fences

Historic Features



Adens Store

**Building Advertising**

Historic Features



Doc Kettle's Office

**Boardwalk**

Historic Features



Historic Features



Orchards

Historic Features



Boones Ferry Road

**Open Road/No Curb/ Mature Trees**

Historic Features



Second Wilsonville School



Wilsonville Church

**Bell Tower**

## Historic Features



Boones Landing

**Concrete Road**

## Historic Features



Historic Features



Historic Features



Boones Ferry

Fence and Dock

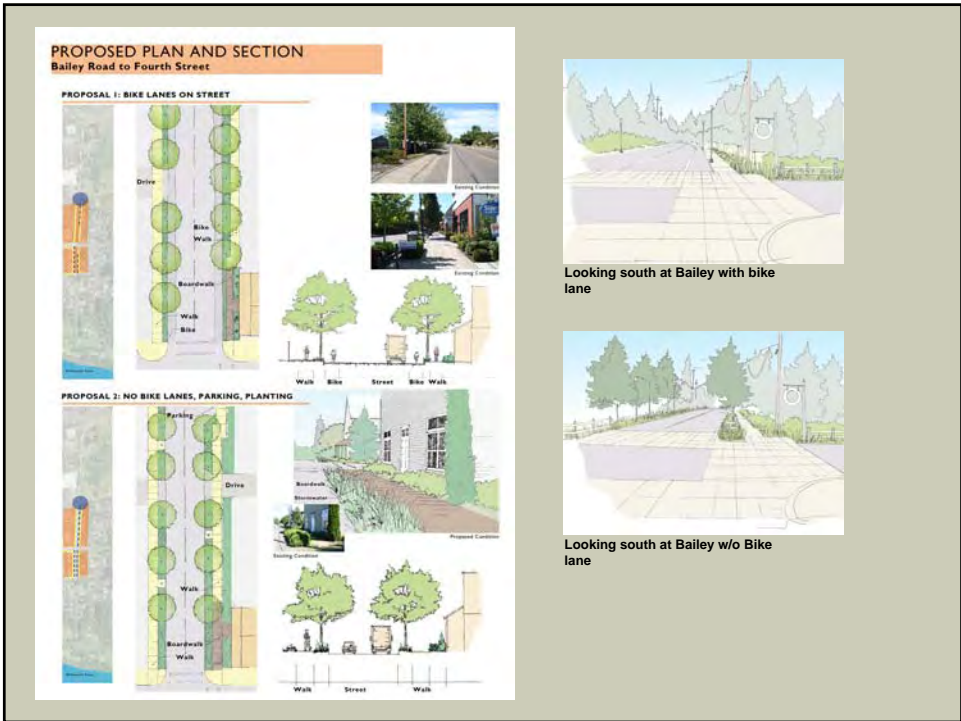
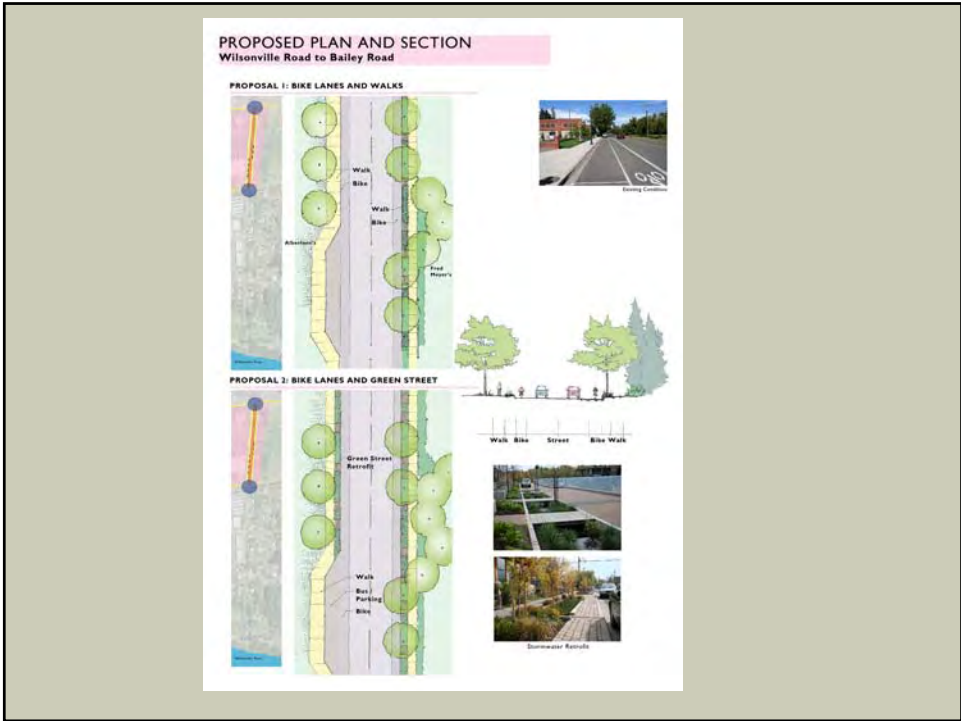
# General Development Concept 1

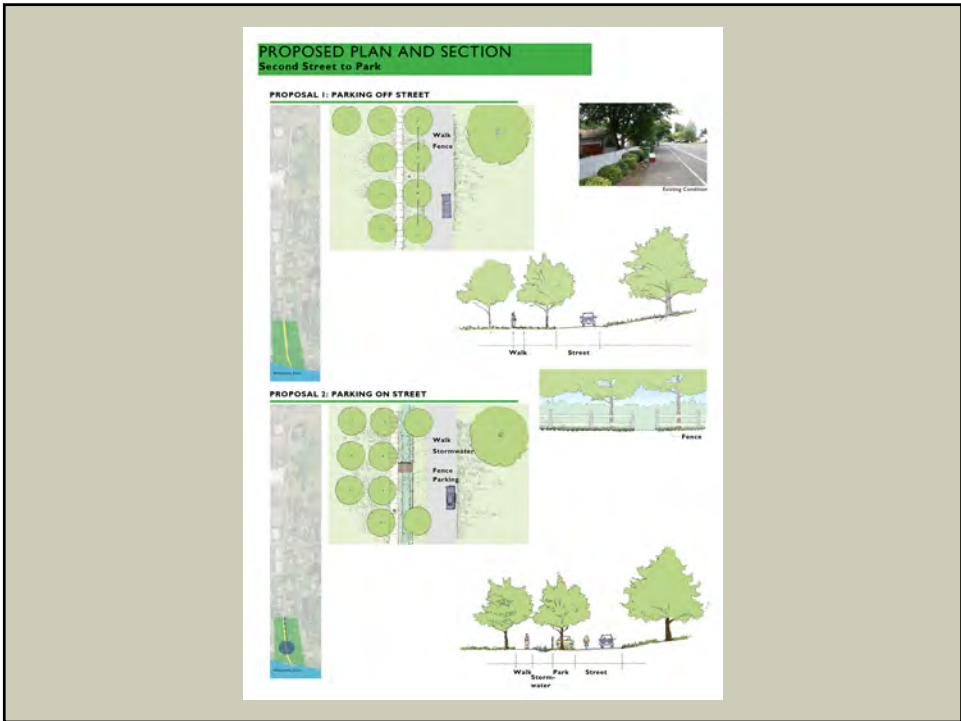
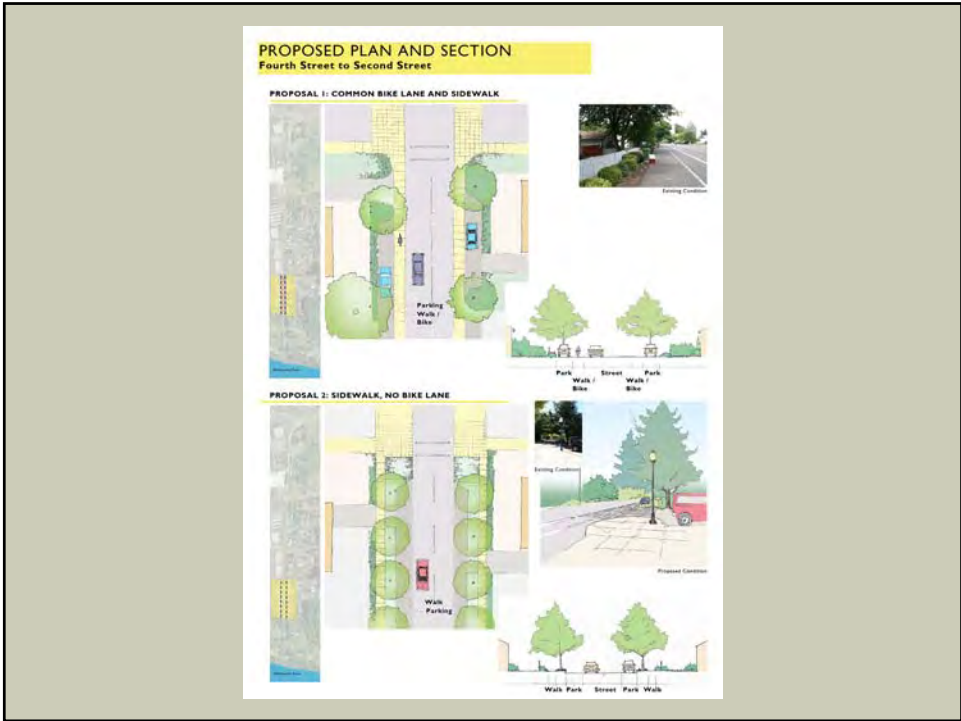


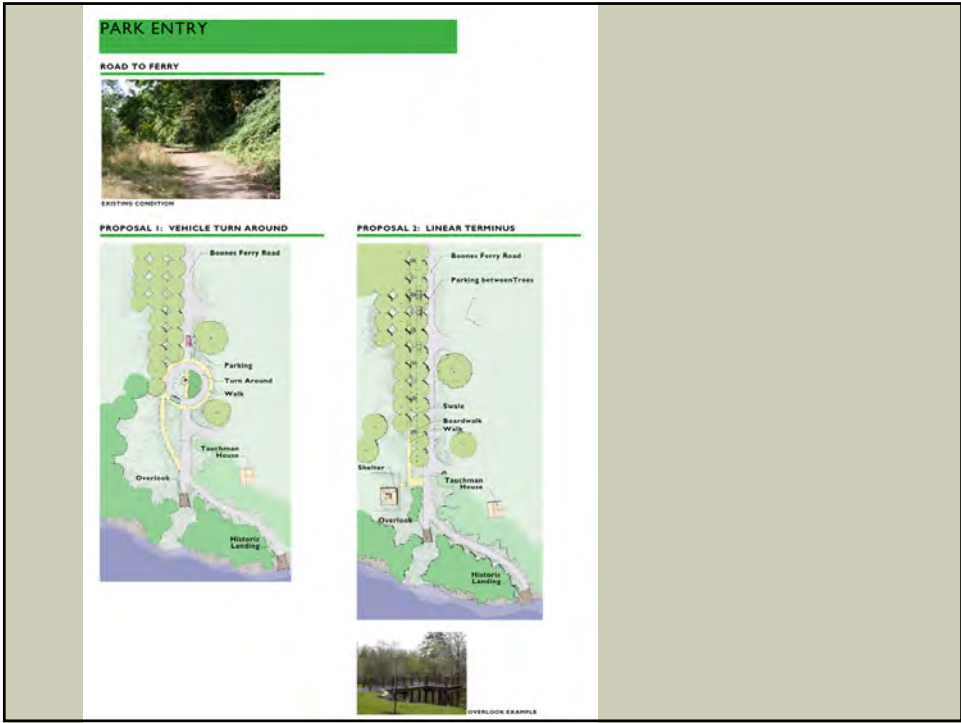
# General Development Concept 2

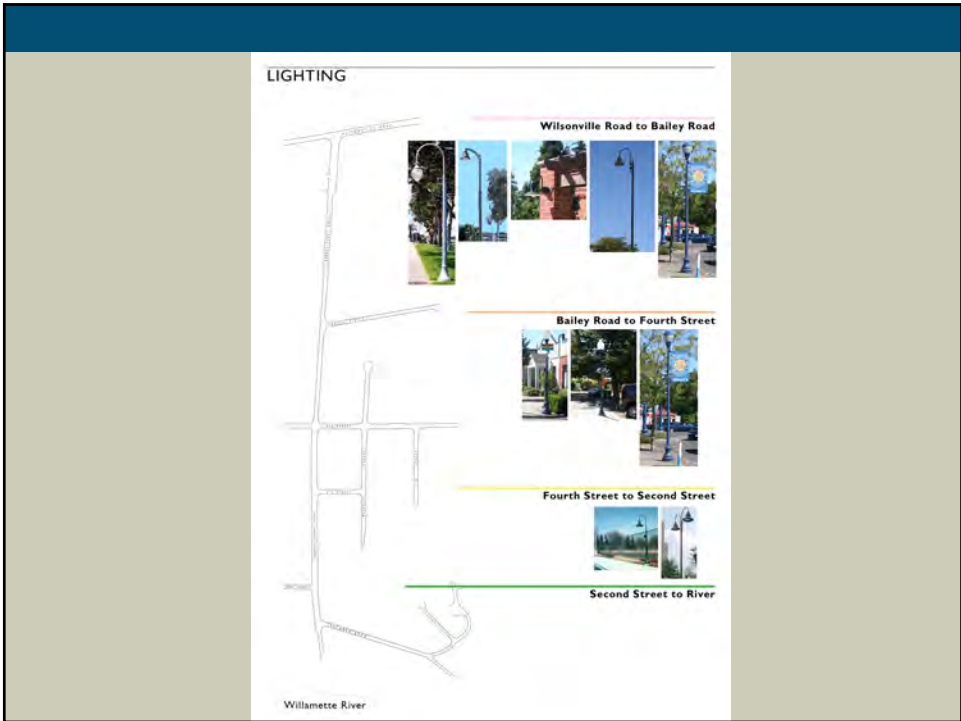
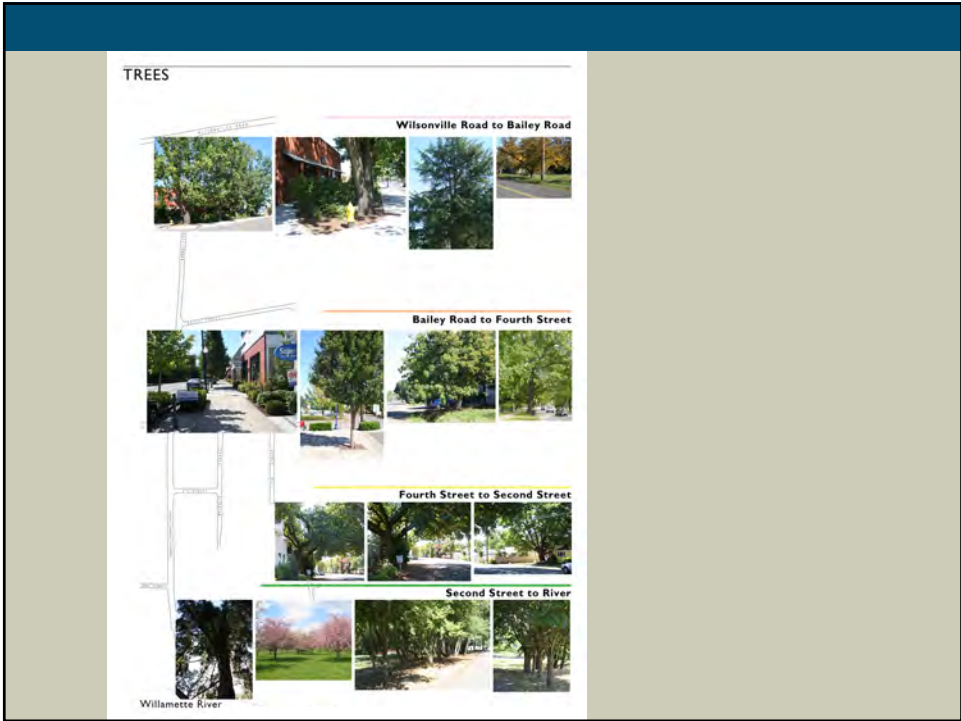


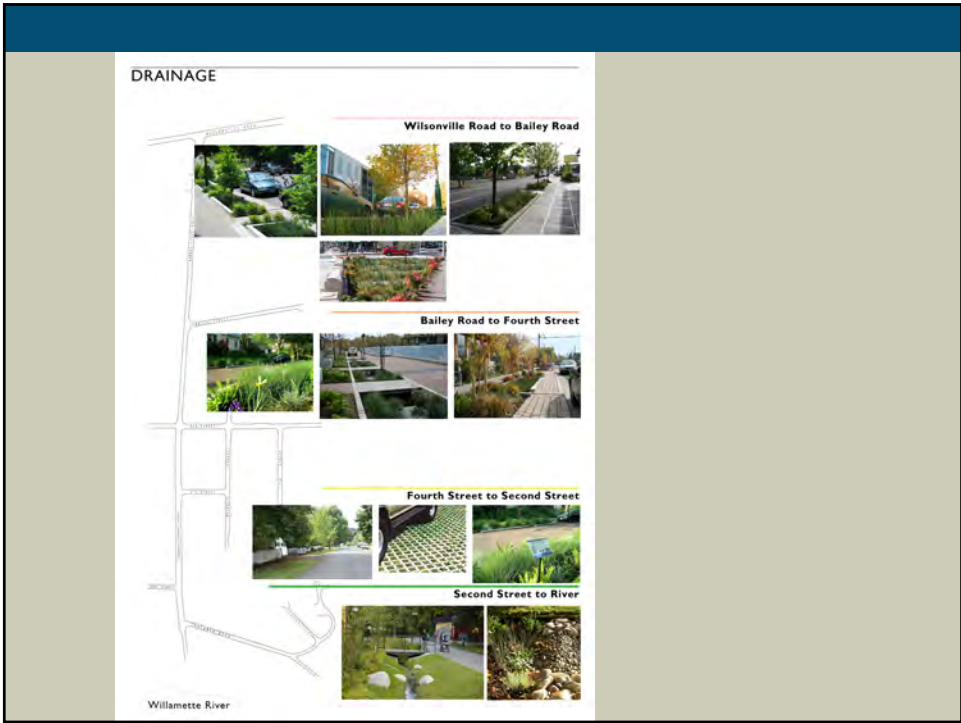
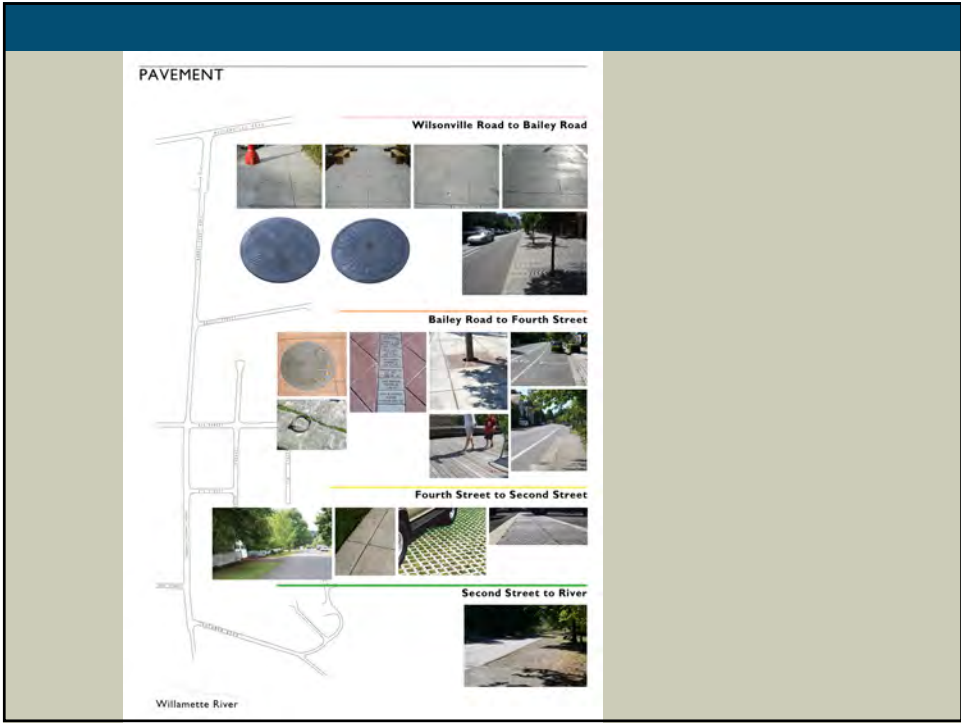


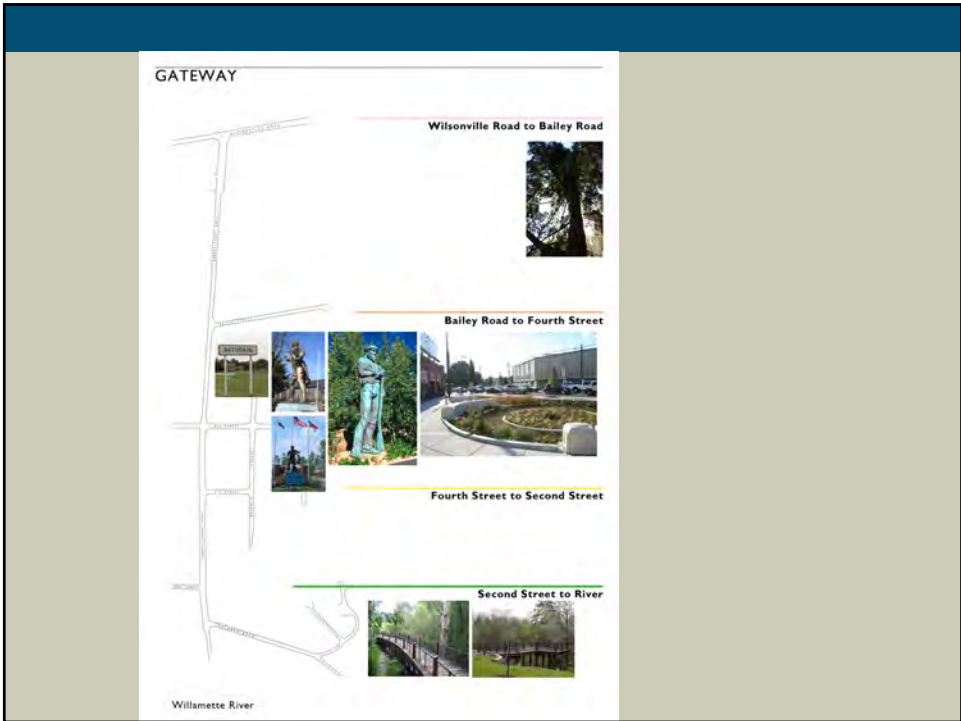
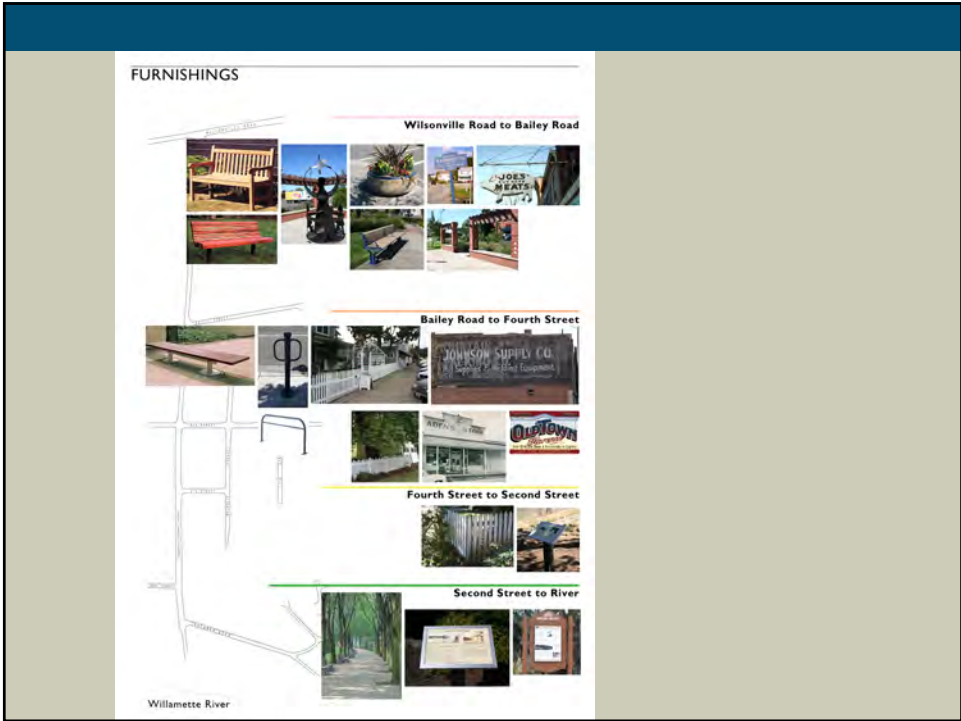












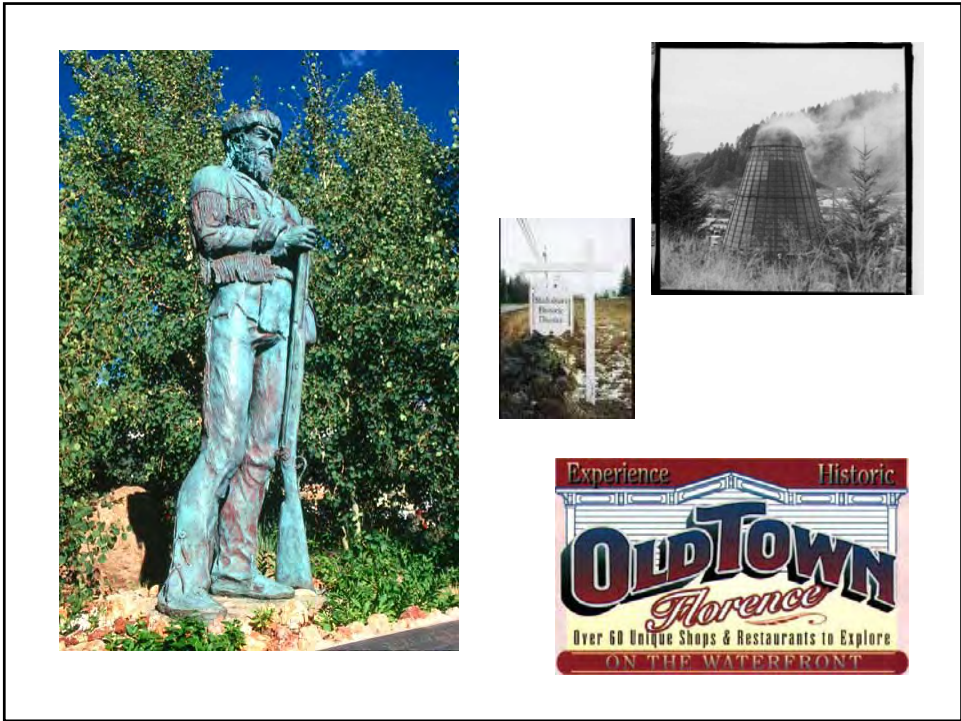
## Gateways











## Discussion Questions

### Questions

1. **Should Old Town Wilsonville be more like Old Town Sherwood or Willamette?**

2. **Wilsonville Rd to Bailey**

Should intersection be used to lead people to the historic district?

Comments on concepts

3. **Bailey to fourth**

Removing bike lane and adding planting can scale down the street. Is that a good idea?

What would be appropriate for gateway into the old town district?

Other comments on concepts

4. **Fourth to Second**

Should the sidewalk be close to homes or close to the street?

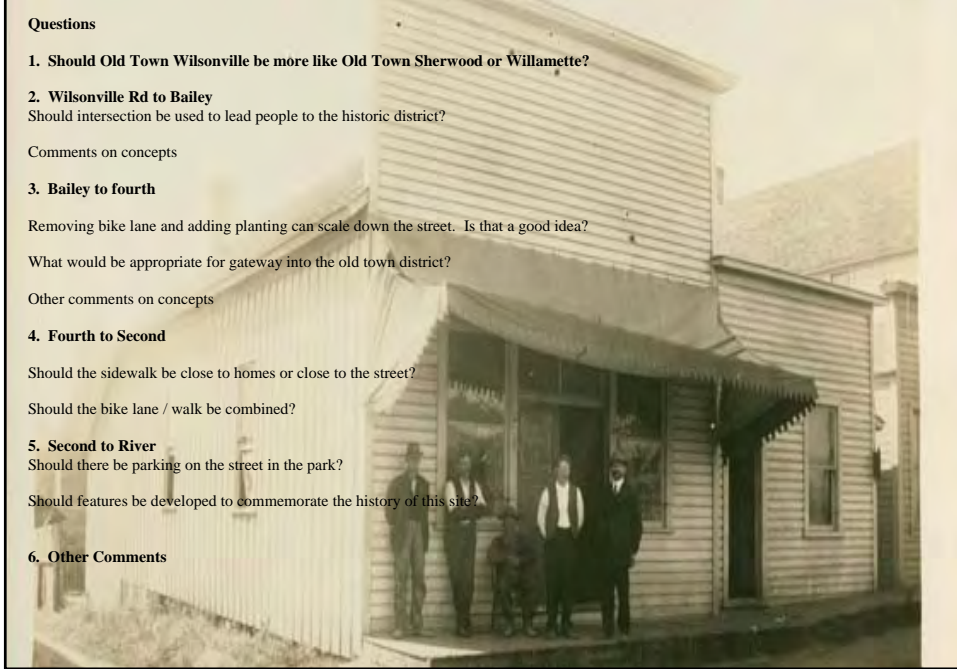
Should the bike lane / walk be combined?

5. **Second to River**

Should there be parking on the street in the park?

Should features be developed to commemorate the history of this site?

6. **Other Comments**



## Boones Ferry Road



# **APPENDIX D**

## **Proposed Capital Projects Preliminary Cost Estimates June 2008**

### **Subsection A: Introduction**

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## **INTRODUCTION, SUMMARY, AND ASSUMPTION SECTION**

### INTRODUCTION

The preliminary cost estimates provided in this Appendix were prepared based on the information and recommendations provided by City staffs, Drafted Old Town Neighborhood Plan (OTNP) dated October 2007, and the Boones Ferry Road Streetscape Project technical memorandum dated February 2008 by Nevue Ngan Associates.

The cost estimates include the streetscape improvements for Boones Ferry Road, Magnolia Street and all cross streets, Fir Street and all cross streets, Alternate access road from Bailey to wastewater treatment plant, Brown Road south extension (Option A to Fifth St and Option B to Bailey Street), Kinsman Road south extension to potential future Brown Road, Boones Ferry Park west improvements, vegetative screening between I-5 and Old Town, pedestrian and bike trail connection, and boat dock.

### SUMMARY

A summary table for the preliminary costs associated with the items mentioned in the introduction section above is attached at the end of this section. The summary table also provides references to the OTNP, and Nevue Ngan technical memo. An overall map indicating the general location of the improvements is also provided at the end of this section, see Map A1.

### ASSUMPTIONS

Below list the general assumptions and/or information used to prepare the preliminary cost estimates at the planning stage level for the OTNP. A detailed breakdown of items used to establish the cost can be referred in the cost estimates sub sections of the Appendix.

#### **Boones Ferry Road Streetscape Assumptions**

- Boones Ferry Road preliminary streetscape cost estimates are split into five segments as outlined in the Boones Ferry Road Streetscape plan prepared by Nevue Ngan Associates dated February 2008.
- Cost estimates from Wilsonville Road to Fourth Street have been estimated both as Major and Minor street classification for comparison.
- From Fourth Street to pedestrian bridge have been estimated based on Residential street classification with on-street parking.
- Major street cross sections with on-street parking based on City of Wilsonville 2003 Transportation System Plan (TSP), Figure 4.19.
- Minor street cross sections with on-street parking based on TSP, Figure 4.17.
- Assumed "DARKSKY" street lights as recommended by City staffs.
- Assumed majority of the existing storm, sanitary and water system to be replaced and/or upgraded in size, using recommendation provided by the City of Wilsonville (see subsection G), 2001 Wastewater Collection System Master Plan (WCSPM), and 2001 Stormwater Master Plan (SMP).



- Assumed Boones Ferry Pedestrian Bridge improvements will start at end of Boones Ferry Road Improvements and extend across the river to the south side of the Willamette River where it will join with the proposed community walkway and bikeway as outline in Map 10.1 of the OTNP. A cost for the bridge improvement is not included with this study.
- Street pavement section was based upon Wilsonville Road Phase 2B & 3B pavement sections and Boberg Street section. Sections include 2" of ½" and 4" of ¾" dense graded level "3" AC pavements over 13" of aggregate base.
- Traffic control labor price was based upon 4 flaggers per 8 hour day for 80 days.
- Assumed full road reconstruction. The estimates also include the cost that would be paid by Fred Meyer's public improvements which includes 24-feet from face of curb to roadway and all east side public improvements (sidewalks, swale, landscaping strip, and parking).

#### Old Town Residential Streets Streetscape Assumptions

- Old Town residential streetscape based on recommendation presented in the OTNP.
- Residential streets for Old Town have been subdivided into three distinct cases (Case A, Case B, and Case C).
  - Case A1, existing road with roughly 30-feet of existing pavement to be grind and overlaid by 4-inches of new pavement. Provide gravel parking strip on both sides of the road as recommended in the OTNP. Improve/extend the existing waterline to 18-inch water main.
  - Case A2, same as Case A1 with 8-inch water main improvements.
  - Case B, existing road with roughly 10-20 feet of existing pavement to be grind and overlaid by 4-inches of new pavement. Assumed an average of 15-feet of existing pavement to remain and be ground, and add additional 9-feet of pavement to provide a total of two 12-foot travel lanes. Provide gravel parking strip on both sides of the road as recommended in the OTNP.
  - Case C, new residential road section.
- The existing right-of-way for residential streets located in Old Town is estimated at 60-foot based on City of Wilsonville digital drawing and verified by Clackamas County Quarter Section Map (03S01W23AC).
- The residential street standards is based on the 2003 City of Wilsonville Transportation System Plan (TSP) for right-of-way varying from 47-feet to 59-feet (TSP, Figure 4.14 and Figure 4.15)
- Provide an 8-foot wide gravel parking strip on both sides of the street as recommended in the OTNP.
- Assumed majority of the existing storm, sanitary and water system to be replaced and/or upgraded in size using recommendation provided by the City of Wilsonville as provided in this Appendix under subsection G, 2001 Wastewater Collection System Master Plan (WCSMP), and 2001 Stormwater Master Plan (SMP).
- The estimates was prepared by MacKay & Sposito, Inc. (M&S), in September 2004, revised in January 2007, and revised again in April 2008, and is based upon topographic drawings and aerial photographs received from the City of Wilsonville, as well as a proposed lane configuration drawing prepared by Group Mackenzie, and Fred Meyer layout.

- The estimate was prepared by M&S in April 2008 and is based upon topographic drawings, and aerial photographs.
- Street pavement section was based upon Wilsonville Road Phase 2B & 3B pavement sections and Boberg Street section. Sections include 2" of ½" and 4" of ¾" dense graded level "3" AC pavements over 13" of aggregate base.
- Traffic control labor price was based upon 2 flaggers per 8 hour day for 40 days.
- This project may require the acquisition of land for right-of-way purposes. A cost for this land acquisition was not included in the estimates.

#### Brown Road and Kinsman Road Extension Assumptions

- Brown Road
  - Cost estimates have been provided for two alternatives. Option A alternative assumed Brown Road to extend to Fifth Street, and Option B alternative assumed Brown Road to extend to Bailey Street.
  - Two lanes Minor Collector road classification (TSP, Figure 4.7 & 4.8) with an additional 14-foot center median for turn lanes for conservative cost estimates. Potential future alignment shown in exhibit maps provided in the appendix section has been established per City of Wilsonville recommendation.
  - Wetland impact and mitigation may be required for Brown Road to cross existing Seely Creek.
  - Assumed railroad surface crossing to be improved as part of the Brown Road extension per each alternatives.
  - Assumed concrete street to match Kinsman Road concrete street design and as recommended by City staff.
  - Option A
    - The estimate was prepared by M&S in December 2005, revised in January 2007, and revised again in February 2008.
    - Full width street improvements from the intersection of Wilsonville Road and Brown Road to the intersection of Boones Ferry Road and Fifth Street, (approximately 3,550 lineal feet).
    - Include traffic signalization, and underground utilities.
    - Street section were based upon a 77-foot right-of-way, 50-foot roadway width with 8" of 1-1/2" base rock, 2" of 3/4" top rock and 7" concrete pavement.
    - Traffic control labor price was based upon 2 flaggers per 8 hour day for 100 days.
    - Grading quantities were based on an assumed excavation depth of 2-feet over the project area from right-of-way to right-of-way.
    - A right-of-way cost is included in the estimates. The approximated right-of-way necessary is 275,000 square feet, and area of slope and/or temporary construction easements is estimated at 35,500 square feet.
  - Option B
    - The estimate was prepared by M&S in December 2006, revised in January 2007, and revised again in February 2008.
    - Full width street improvements from the intersection of Wilsonville Road and Brown Road to the intersection of Boones Ferry road and Bailey Street, (approximately 3,400-feet).

- Include traffic signalization, and underground utilities.
  - Street section were based upon a 77-foot right-of-way, 50-foot roadway width with 8" of 1-1/2" base rock, 2" of 3/4" top rock and 7" concrete pavement.
  - Traffic control labor price was based upon 2 flaggers per 8 hour day for 100 days.
  - Grading quantities were based on an assumed excavation depth of 2-feet over the project area from right-of-way to right-of-way.
  - A right-of-way cost is included in the estimates. The approximated right-of-way necessary is 272,000 square feet, and area of slope and/or temporary construction easements is estimated at 34,000 square feet.
- Kinsman Road
    - Two lanes Minor Collector road classification (TSP, Figure 4.7 & 4.8) with an additional 14-foot center median for turn lanes for conservative cost estimates. Potential future alignment shown in exhibit maps provided in the appendix section has been established per City of Wilsonville recommendation.
    - Wetland impact and mitigation may be required for Kinsman Road to cross existing Seely Creek.
    - Assumed concrete street for Kinsman Road extension to match the existing Kinsman Road north of Wilsonville Road and as recommended by City staff.
    - The estimate was prepared by M&S in April 2008.
    - Full width street improvements from the intersection of Wilsonville Road and Kinsman Road to the intersection of future Kinsman Road and future Brown Road south extension.
    - Include traffic signalization, and underground utilities.
    - Street section were based upon a 77-foot right-of-way, 50-foot roadway width with 8" of 1-1/2" base rock, 2" of 3/4" top rock and 7" concrete pavement.
    - Traffic control labor price was based upon 2 flaggers per 8 hour day for 100 days.
    - Grading quantities were based on an assumed excavation depth of 2-feet over the project area from right-of-way to right-of-way.
    - Right-of-way costs are included in this estimate.

#### Park and Trails Assumptions

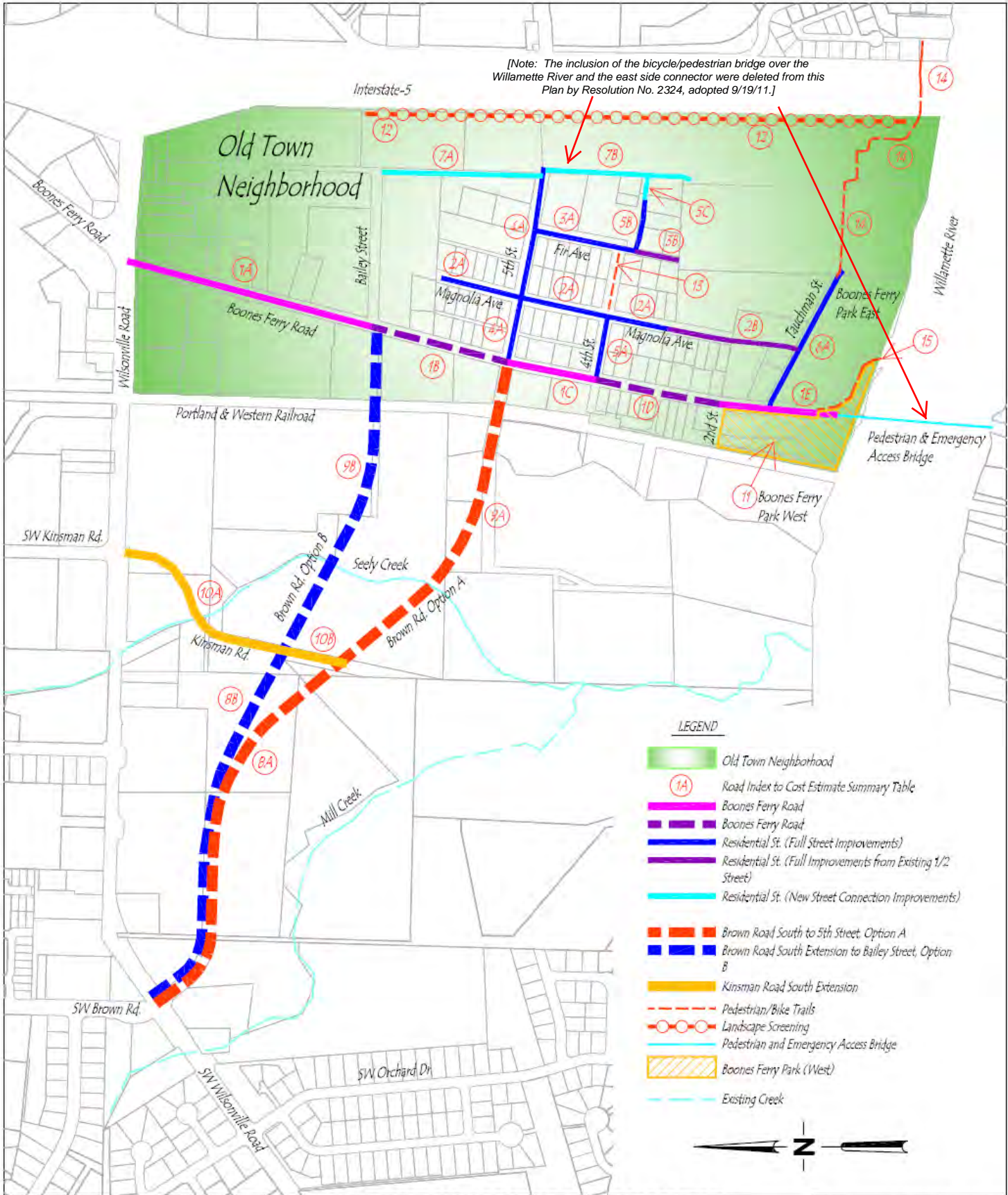
- Boones Ferry Park (West)
  - Cost based on acreage for typical park improvements as established by MacKay & Sposito landscape architect. Detailed lists of items used to establish the cost per acreage have been included in this package. The cost per acreage includes items such as playground, benches, tables, trail, irrigation, power, water, plantings, bike rack, trash, sports court, parking lot, shelter, restroom, and drinking fountain. The improvement will be for Boones Ferry Park west of Boones Ferry Road with an estimated net acreage of 3.88 acres.
- Vegetative screening
  - Between I-5 and Old Town to be accomplished by planting dense evergreen trees of 3" caliper every 10-foot for conservative rough estimates. Cost based on recent planting material for the Baber Street Improvement from Boberg to Kinsman Road.

- Trails
  - Improve trail near existing mobile home park at Tauchman Street to east end of trail under I-5 overpass. Pedestrian footbridge located east of the mobile home park to remain, and assumed it is not accessible to maintenance and emergency vehicles. Maintenance and emergency vehicles can access from the Waste Water Treatment Plant and along ODOT property.
  - Improve trail between Fir and Magnolia at Fifth Street.
  - Trail cost is based on an assumed 12-foot wide travel path. The 12-foot wide travel path would provide enough room for maintenance and emergency vehicles.
- Boat dock
  - Cost based on existing projects listed in the Oregon State Marine Board website ([www.oregon.gov/OSMB/news/2003/6-26Facilities.html](http://www.oregon.gov/OSMB/news/2003/6-26Facilities.html)).
- Costs for park and trails are based on recommendations outlined in Chapter 10 of the OTNP.

#### General Assumptions

- Unit prices shown in this estimate were based upon current bid prices received by this office without increase in inflation.
- The following items were not included in the cost estimates.
  - Any staking costs
  - Geotechnical engineering
  - Wetland or other environmental permits
  - Any agency fees for application, review, system development, or impact fees.
  - Costs for permanent storm water quality or quantity.
  - Any fences or wall.
  - Any structure design.

[Note: The inclusion of the bicycle/pedestrian bridge over the Willamette River and the east side connector were deleted from this Plan by Resolution No. 2324, adopted 9/19/11.]



**OLD TOWN PRELIMINARY COST ESTIMATES**  
**OLD TOWN NEIGHBORHOOD**  
**IMPROVEMENTS PLAN AND POTENTIAL**  
**ROAD EXTENSION** Scale: 1" = 600'

M&S PROJECT #: 14892  
 DRAWN BY: EH/MW  
 CHECKED BY: RP  
 DATE: 06/18/08  
 SHEET NO. A1



**PRELIMINARY COST ESTIMATE SUMMARY TABLE  
FOR OLD TOWN NEIGHBORHOOD STREETScape IMPROVEMENTS**

ROAD INDEX	APPENDIX SUB SECTION	BOONES FERRY ROAD	NOTE	REFERENCE	OPTION A: MAJOR ROAD CLASSIFICATION	OPTION B: MINOR ROAD CLASSIFICATION
1A	B2 & B3	Wilsonville Road to Bailey Street (City's Portion)	Major/Minor, see option	Nevue Ngan Technical Memo, Feb. 2008, pg. 9 & 24, Old Town Neighborhood Plan Oct. 2007, Ch. 8, Pg. 2 & 5	\$1,001,000.00	\$821,000.00
1A	B2 & B3	Wilsonville Road to Bailey Street (Fred Meyer Portion)	Major/Minor, see option	Nevue Ngan Technical Memo, Feb. 2008, pg. 9 & 24, Old Town Neighborhood Plan Oct. 2007, Ch. 8, Pg. 2 & 6	\$1,909,000.00	\$1,882,000.00
1B	B4 & B5	Bailey Street to Fifth Street	Major/Minor, see option	Nevue Ngan Technical Memo, Feb. 2008, pg. 13 & 25, Old Town Neighborhood Plan, Oct. 2007, Ch. 8, Pg. 2 & 5	\$1,716,000.00	\$1,441,000.00
1C	B6 & B7	Fifth Street to Fourth Street	Major/Minor, see option	Nevue Ngan Technical Memo, Feb. 2008, pg. 13 & 25, Old Town Neighborhood Plan, Oct. 2007, Ch. 8, Pg. 2 & 5	\$1,082,000.00	\$916,000.00
1D	B8	Fourth Street to Second Street	Residential Street Classification	Nevue Ngan Technical Memo, Feb. 2008, pg. 17 & 26, Old Town Neighborhood Plan, Oct. 2007, Ch. 8, Pg. 2 & 5	\$932,000.00	\$932,000.00
1E	B9	Second Street to Boones Ferry Bridge	Residential Street Classification	Nevue Ngan Technical Memo, Feb. 2008, pg. 21 & 27, Old Town Neighborhood Plan Oct. 2007, Ch. 8, Pg. 2 & 5	\$728,000.00	\$728,000.00
<b>(\$2008) TOTAL FOR BOONES FERRY ROAD STREETScape IMPROVEMENTS</b>					<b>\$7,368,000.00</b>	<b>\$6,720,000.00</b>

ROAD INDEX	APPENDIX SUB SECTION	OLD TOWN RESIDENTIAL STREETS	NOTE	REFERENCE	PRELIMINARY ESTIMATES
2A	C2	Magnolia Avenue, from cul-de-sac to 280-feet south of Fourth Street	Residential street standards with gravel parking and 30-feet of existing pavement	Old Town Neighborhood Plan, Oct. 2007, Ch. 8, Pg. 2 & 5	\$1,306,000.00
2B	C3	Magnolia Avenue, from 280-feet south of Fourth Street to Tauchman Street	Residential street standards with gravel parking and 15-feet of existing pavement	Old Town Neighborhood Plan, Oct. 2007, Ch. 8, Pg. 2 & 5	\$1,010,000.00
3A	C2	Fir Street, from Fifth Street to Fourth Street	Residential street standards with gravel parking and 30-feet of existing pavement	Old Town Neighborhood Plan, Oct. 2007, Ch. 8, Pg. 2 & 5	\$571,000.00
3B	C3	Fir Street, from south of Fourth Street to dead end	Residential street standards with gravel parking and 15-feet of existing pavement	Old Town Neighborhood Plan, Oct. 2007, Ch. 8, Pg. 2 & 5	\$257,000.00
4A	C2	Fifth Street, from Boones Ferry Road to Dead End	Residential street standards with gravel parking and 30-feet of existing pavement	Old Town Neighborhood Plan, Oct. 2007, Ch. 8, Pg. 2 & 5	\$1,222,000.00
5A	C2	Fourth Street, from Boones Ferry Road to Magnolia Avenue	Residential street standards with gravel parking and 30-feet of existing pavement	Old Town Neighborhood Plan, Oct. 2007, Ch. 8, Pg. 2 & 5	\$364,000.00
5B	C2	Fourth Street, east of Fir Street	Residential street standards with gravel parking and 30-feet of existing pavement	Old Town Neighborhood Plan, Oct. 2007, Ch. 8, Pg. 2 & 5	\$583,000.00
5C	C4	Connection from Fourth Street to Alternate Access near treatment plant	Potential new road with residential street standards with gravel parking	Old Town Neighborhood Plan, Oct. 2007, Ch. 8, Pg. 2 & 5	\$253,000.00
6A	C2	Tauchman Street, from Boones Ferry Road to road fork	Residential street standards with gravel parking and 30-feet of existing pavement	Old Town Neighborhood Plan, Oct. 2007, Ch. 8, Pg. 2 & 5	\$887,000.00
7A	C4	Alternate Access by existing church (9201 SW 5th St), from Bailey Street to Fifth Street	Potential new road with residential street standards with gravel parking	Old Town Neighborhood Plan, Oct. 2007, Ch. 6, pg. 9, Ch. 8, pg. 2 & 5	\$1,448,000.00
7B	C4	Alternate Access to wastewater treatment plant, from Fifth Street to north of wastewater treatment plant	Potential new road with residential street standards with gravel parking	Old Town Neighborhood Plan, Oct. 2007, Ch. 6, pg. 9, Ch. 8, pg. 2 & 5	\$1,553,000.00
<b>(\$2008) TOTAL FOR OLD TOWN STREETScape IMPROVEMENTS</b>					<b>\$9,454,000.00</b>

*[Note: The inclusion of Projects 7A & 7B, were deleted from this Plan by Resolution No. 2324, adopted 9/19/11.]*

ROAD INDEX	APPENDIX SUB SECTION	BROWN ROAD SOUTH EXTENSION	NOTE	REFERENCE	OPTION A:	OPTION B:
					WILSONVILLE ROAD TO FIFTH STREET	WILSONVILLE ROAD TO BAILEY STREET
8A & 8B	D2 & D3	Wilsonville Road to Kinsman Road South Extension	Minor Road Collector	Old Town Neighborhood Plan, Oct. 2007, Ch. 6, pg. 7	\$5,637,000.00	\$5,726,000.00
9A & 9B	D2 & D3	Kinsman Road South Extension to Fifth Street & Bailey Street	Minor Road Collector	Old Town Neighborhood Plan, Oct. 2007, Ch. 6, pg. 7	\$7,213,000.00	\$6,577,000.00
<b>(\$2008) TOTAL FOR BROWN ROAD SOUTH EXTENSION</b>					<b>\$12,850,000.00</b>	<b>\$12,303,000.00</b>

ROAD INDEX	APPENDIX SUB SECTION	KINSMAN ROAD SOUTH EXTENSION	NOTE	REFERENCE	PRELIMINARY ESTIMATES	
10A	E2	Wilsonville Road to Brown Road (Option B)	Minor Road Collector	Old Town Neighborhood Plan, Oct. 2007, Ch. 8, pg. 2		\$3,792,000.00
10B	E2	Brown Road (Option B) to Brown Road (Option A)	Minor Road Collector	Old Town Neighborhood Plan, Oct. 2007, Ch. 8, pg. 2		\$2,095,000.00
<b>(\$2008) TOTAL FOR KINSMAN ROAD SOUTH EXTENSION</b>						<b>\$5,887,000.00</b>

ROAD INDEX	APPENDIX SUB SECTION	PARK AND TRAILS	NOTE	REFERENCE	PRELIMINARY ESTIMATES	
11	F2	Boones Ferry Park (West)	Roughly 3.88 Acres	Old Town Neighborhood Plan, Oct. 2007, Ch. 10, pg. 1		\$939,000.00
12	F2	Vegetative screening between I-5 and Old Town	3" caliper evergreen trees	Old Town Neighborhood Plan, Oct. 2007, Ch. 10, pg. 3		\$161,000.00
13	F2	Sidewalk Trail on Fourth Street between Magnolia and Fir St.	12-foot wide path, accessible for emergency and maintenance vehicles	Old Town Neighborhood Plan, Oct. 2007, Ch. 8, pg. 9		\$21,000.00
14	F2	Pedestrian/bike trail connection improvements from Tauchman Street to east side of freeway	12-foot wide path, accessible for emergency and maintenance vehicles	Old Town Neighborhood Plan, Oct. 2007, Ch. 8, pg. 9		\$103,000.00
15	F2	Boat dock	Boat Ramp and loading dock for non motorized watercraft	Old Town Neighborhood Plan, Oct. 2007, Ch. 8, pg. 11		\$200,000.00
<b>(\$2008) TOTAL FOR PARK AND TRAILS</b>						<b>\$1,424,000.00</b>



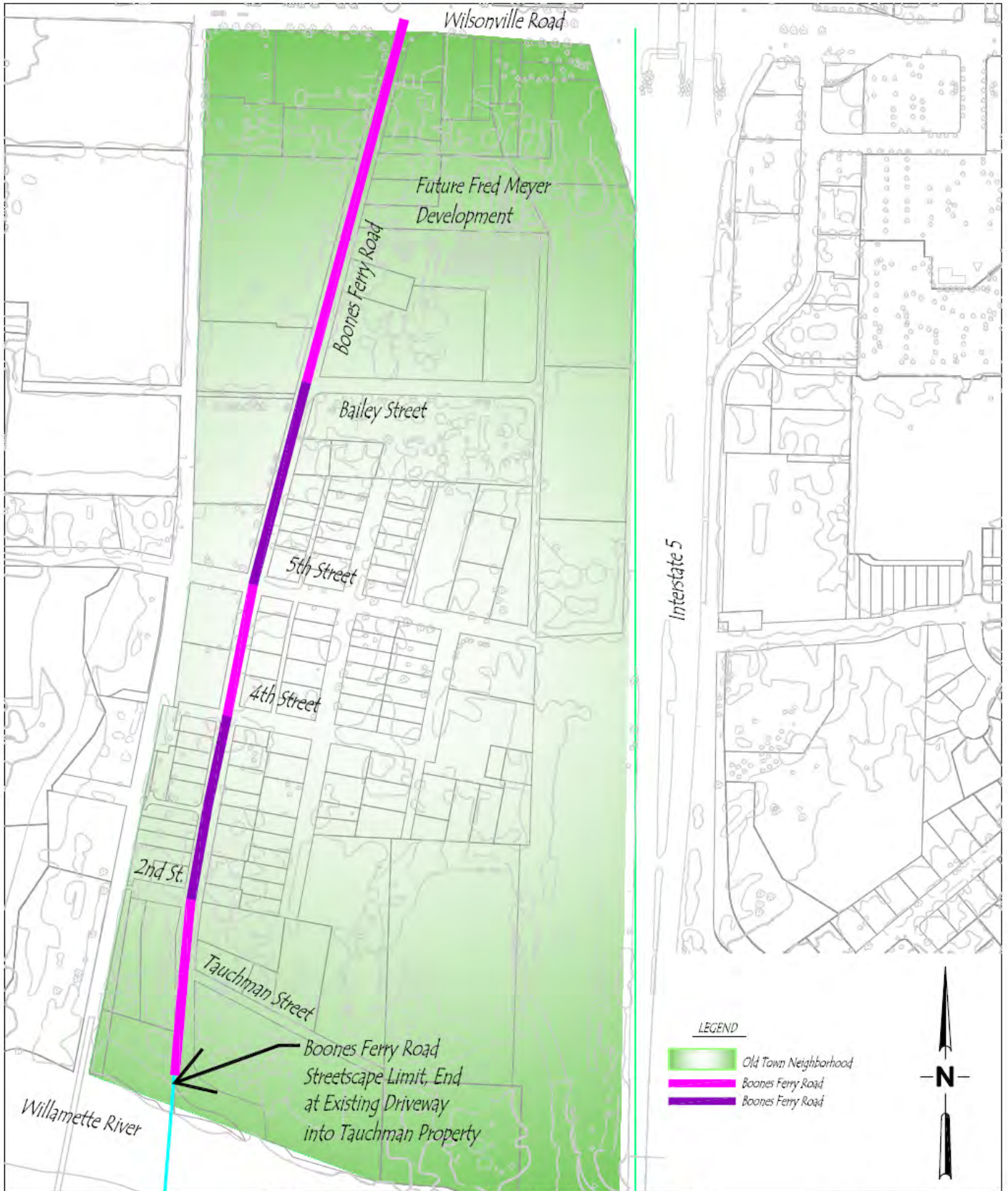


# **APPENDIX D**

## **Proposed Capital Projects Preliminary Cost Estimates June 2008**

### **Subsection B: Boones Ferry Road Streetscape Improvements**





**OLD TOWN PRELIMINARY COST ESTIMATES**

M&S PROJECT #: 14892

**OLD TOWN**

DRAWN BY: EH/MW

**BOONES FERRY ROAD**

CHECKED BY: RP

DATE: 06/18/08

SHEET NO. B1

Scale: 1" = 500'



MacKay & Sposlto, Inc.

**SUBSECTION B2****PROJECT:**

**BOONES FERRY STREET IMPROVEMENTS - PRELIMINARY COST ESTIMATE  
FROM WILSONVILLE ROAD TO BAILEY STREET  
MAJOR COLLECTOR WITH PARKING  
CITY OF WILSONVILLE / FRED MEYER**

**OWNER:****DATE: APRIL 16, 2008**

ITEM	QUANTITY		UNIT	DESCRIPTION	UNIT PRICE	ENGINEER'S EST.		TOTAL
	FRED MEYER	COW				SUB TOTAL FRED MEYER	SUB TOTAL COW	

**STREET WORK**

1.			L.S.	Mobilization		\$78,437.00	\$49,373.00	\$127,810.00
2.			L.S.	Temp. Protection & Direction of Traffic		\$17,500.00	\$17,500.00	\$35,000.00
3.	1,100	1,100	HRS.	Flaggers	\$40.00	\$44,000.00	\$44,000.00	\$88,000.00
4.	90	90	Day	Portable Changeable Message Signs	\$200.00	\$18,000.00	\$18,000.00	\$36,000.00
5.			L.S.	Watering		\$3,000.00	\$2,000.00	\$5,000.00
6.			L.S.	Clearing and Grubbing		\$17,500.00	\$17,500.00	\$35,000.00
7.	2,010	4,290	C.Y.	Unclassified Excavation	\$14.00	\$28,140.00	\$60,060.00	\$88,200.00
8.	1,070	1,750	Ton	3/4" Dense Graded Level 3 A.C Pavement	\$48.00	\$51,360.00	\$84,000.00	\$135,360.00
9.	350	580	Ton	1/2" Dense Graded Level 3 A.C Pavement	\$53.00	\$18,550.00	\$30,740.00	\$49,290.00
10.	1,020	1,670	C.Y.	Aggregate Base	\$32.00	\$32,640.00	\$53,440.00	\$86,080.00
11.		450	L.F.	Asphalt Pavement Cutting	\$2.00		\$900.00	\$900.00
12.	250		C.Y.	Rock Excavation (If Nec.)	\$35.00	\$8,750.00		\$8,750.00
13.	100		C.Y.	Import Subgrade Stab. Rock (If Nec.)	\$25.00	\$2,500.00		\$2,500.00
14.	3,320	5,460	S.Y.	Subgrade Geotextile (If Nec.)	\$1.00	\$3,320.00	\$5,460.00	\$8,780.00
<b>TOTAL STREET WORK</b>						<b>\$323,697.00</b>	<b>\$382,973.00</b>	<b>\$706,670.00</b>

**CONCRETE WORK**

1.	1,090	1,260	L.F.	Curb and Gutter, Concrete Curbs	\$12.00	\$13,080.00	\$15,120.00	\$28,200.00
2.	1,080	2,040	S.F.	Standard Concrete Commercial Driveway	\$4.50	\$4,860.00	\$9,180.00	\$14,040.00
3.	10,400	6,090	S.F.	Concrete Walks (incl. Bus Stop Pads)	\$3.50	\$36,400.00	\$21,315.00	\$57,715.00
<b>TOTAL CONCRETE WORK</b>						<b>\$54,340.00</b>	<b>\$45,615.00</b>	<b>\$99,955.00</b>

## STORM SEWER

1.	120	300	L.F.	12" C-900 Storm Lateral (Imp. Backfill)	\$34.00	\$4,080.00	\$10,200.00	\$14,280.00
2.		745	L.F.	24" Storm Main	\$60.00		\$44,700.00	\$44,700.00
3.		5	EA.	Curb Inlet (Type CG-30)	\$1,100.00		\$5,500.00	\$5,500.00
4.		1	EA.	Curb Inlet (Type CG-48)	\$1,300.00		\$1,300.00	\$1,300.00
5.	4		EA.	Ditch Inlet	\$1,200.00	\$4,800.00		\$4,800.00
6.		2	EA.	48" Conc. Stm. MH	\$2,000.00		\$4,000.00	\$4,000.00
7.		1	EA.	Core & Connect to Ex 48" Conc. Stm. MH	\$1,500.00		\$1,500.00	\$1,500.00
8.		1,050	L.F.	Trench Protection	\$5.00		\$5,250.00	\$5,250.00
9.		30	C.Y.	Trench Foundation (If Nec.)	\$30.00		\$900.00	\$900.00
10.		50	C.Y.	Rock Excavation (If Necessary)	\$100.00		\$5,000.00	\$5,000.00
TOTAL STORM SEWER						\$8,880.00	\$78,350.00	\$87,230.00

## SANITARY

1.	1,200		L.F.	8 Inch Sanitary Sewer Pipe, 10-ft Depth	\$85.00	\$102,000.00		\$102,000.00
2.	3		E.A.	Sanitary Manhole	\$2,000.00	\$6,000.00		\$6,000.00
3.	1,200		L.F.	Trench Protection	\$4.00	\$4,800.00		\$4,800.00
4.	50		C.Y.	Rock Excavation	\$100.00	\$5,000.00		\$5,000.00
TOTAL SANITARY WORK						\$117,800.00		\$117,800.00

## WATER

1.	1,200		L.F.	18 Inch D.I. Potable Water Pipes and Fittings	\$112.00	\$134,400.00		\$134,400.00
2.			L.F.	8" Inch D.I. Potable Water Pipes and Fittings	\$52.00			
3.	4		E.A.	18" Butterfly Valves	\$2,600.00	\$10,400.00		\$10,400.00
4.			E.A.	8" Gave Valves	\$860.00			
5.	2		E.A.	Hydrant Assemblies	\$2,050.00	\$4,100.00		\$4,100.00
6.	1,200		L.F.	Trench Protection	\$1.50	\$1,800.00		\$1,800.00
TOTAL WATER WORK						\$150,700.00		\$150,700.00

## CONDUIT SYSTEM AND VAULTS

1.		150	L.F.	12" PVC C-900 Conduit Sleeves	\$12.00		\$1,800.00	\$1,800.00
2.	1,135		L.F.	Convert Overhead Utilities to Underground	\$100.00	\$113,500.00		\$113,500.00
3.	20		C.Y.	Trench Foundation (If Nec.)	\$30.00	\$600.00		\$600.00
4.	20		C.Y.	Rock Excavation (If Nec.)	\$100.00	\$2,000.00		\$2,000.00
TOTAL CONDUIT SYSTEM AND VAULTS						\$116,100.00	\$1,800.00	\$117,900.00

## EROSION CONTROL

1.	1,200	1,000	L.F.	Sediment Fence, Unsupported	\$3.00	\$3,600.00	\$3,000.00	\$6,600.00
2.	3	8	EA.	Inlet Protection (Type 3)	\$55.00	\$165.00	\$440.00	\$605.00
3.			L.S.	Permanent Seeding, Complete		\$15,000.00	\$200.00	\$15,200.00
TOTAL EROSION CONTROL						\$18,765.00	\$3,640.00	\$22,405.00

## SIGNING AND STRIPING

1.			L.S.	Perm. Striping And Pavement Marking		\$2,500.00	\$2,500.00	\$5,000.00
2.			L.S.	Permanent Sign Installation, Complete		\$1,300.00	\$1,300.00	\$2,600.00
3.			L.S.	Remove Existing Signs		\$250.00	\$250.00	\$500.00
4.			L.S.	Remove and Reinstall Ex. Signs		\$500.00	\$500.00	\$1,000.00
TOTAL SIGNING AND STRIPING						\$4,550.00	\$4,550.00	\$9,100.00

## STREET LIGHTING

1.			L.S.	Roadway Lighting, Complete		\$24,000.00	\$10,500.00	\$34,500.00
TOTAL STREET LIGHTING						\$24,000.00	\$10,500.00	\$34,500.00

## MISCELLANEOUS CONSTRUCTION

1.		3	EA.	Adjust Existing Gas Valves to F.G.	\$200.00		\$600.00	\$600.00
2.			L.S.	Franchise Utility Markers to be Relocated		\$375.00	\$375.00	\$750.00
3.		1	EA.	Adj. Ex. Telephone MH Rim (Minor Adj.)	\$700.00		\$700.00	\$700.00
4.		1	EA.	Adjust Ex. Utility Vault to F.G.	\$700.00		\$700.00	\$700.00
5.		1	EA.	Centerline Monument	\$300.00		\$300.00	\$300.00
6.	1,200	1,000	L.F.	Orange Safety Fence	\$3.00	\$3,600.00	\$3,000.00	\$6,600.00
TOTAL MISCELLANEOUS CONST.						\$3,975.00	\$5,675.00	\$9,650.00

## LANDSCAPING

1.			L.S.	Landscaping, Complete		\$40,000.00	\$10,000.00	\$50,000.00
TOTAL LANDSCAPING						\$40,000.00	\$10,000.00	\$50,000.00

## TOTAL CONSTRUCTION

\$862,807.00	\$543,103.00	\$1,405,910.00
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## MISCELLANEOUS

1.	30,835	5,000	S.F.	Right-of-Way	\$14.00	\$431,690.00	\$70,000.00	\$501,690.00
2.	10,900	7,400	S.F.	Temporary Construction Easement	\$1.00	\$10,900.00	\$7,400.00	\$18,300.00
3.				Design & Construction Costs	30%	\$258,842.10	\$162,930.90	\$421,773.00
4.				Contingency	40%	\$345,122.80	\$217,241.20	\$562,364.00
TOTAL MISCELLANEOUS						\$1,046,554.90	\$457,572.10	\$1,504,127.00

**TOTAL BOONES FERRY ROAD IMPROVEMENTS  
FROM WILSONVILLE ROAD TO BAILEY STREET  
PRELIMINARY COST ESTIMATE**

\$1,909,000.00	\$1,001,000.00	\$2,910,000.00
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**SUBSECTION B3****PROJECT:**

**BOONES FERRY STREET IMPROVEMENTS - PRELIMINARY COST ESTIMATE  
FROM WILSONVILLE ROAD TO BAILEY STREET  
MINOR COLLECTOR WITH PARKING  
CITY OF WILSONVILLE / FRED MEYER**

**OWNER:****DATE: APRIL 16, 2008**

ITEM	QUANTITY		UNIT	DESCRIPTION	UNIT PRICE	ENGINEER'S EST.		TOTAL
	FRED MEYER	COW				SUB TOTAL FRED MEYER	SUB TOTAL COW	

**STREET WORK**

1.			L.S.	Mobilization		\$85,252.00	\$39,753.60	\$125,005.60
2.			L.S.	Temp. Protection & Direction of Traffic		\$17,500.00	\$17,500.00	\$35,000.00
3.	1,100	1,100	HRS.	Flaggers	\$40.00	\$44,000.00	\$44,000.00	\$88,000.00
4.	90	90	Day	Portable Changeable Message Signs	\$200.00	\$18,000.00	\$18,000.00	\$36,000.00
5.			L.S.	Watering		\$3,000.00	\$2,000.00	\$5,000.00
6.			L.S.	Clearing and Grubbing		\$17,500.00	\$17,500.00	\$35,000.00
7.	2,010	3,775	C.Y.	Unclassified Excavation	\$14.00	\$28,140.00	\$52,852.80	\$80,992.80
8.	1,070	1,540	Ton	3/4" Dense Graded Level 3 A.C Pavement	\$48.00	\$51,360.00	\$73,920.00	\$125,280.00
9.	350	510	Ton	1/2" Dense Graded Level 3 A.C Pavement	\$53.00	\$18,550.00	\$27,051.20	\$45,601.20
10.	1,020	1,470	C.Y.	Aggregate Base	\$32.00	\$32,640.00	\$47,027.20	\$79,667.20
11.		450	L.F.	Asphalt Pavement Cutting	\$2.00		\$900.00	\$900.00
12.	250		C.Y.	Rock Excavation (If Nec.)	\$35.00	\$8,750.00		\$8,750.00
13.	100		C.Y.	Import Subgrade Stab. Rock (If Nec.)	\$25.00	\$2,500.00		\$2,500.00
14.	3,320	4,805	S.Y.	Subgrade Geotextile (If Nec.)	\$1.00	\$3,320.00	\$4,804.80	\$8,124.80
<b>TOTAL STREET WORK</b>						<b>\$330,512.00</b>	<b>\$345,309.60</b>	<b>\$675,821.60</b>

**CONCRETE WORK**

1.	1,090	1,260	L.F.	Curb and Gutter, Concrete Curbs	\$12.00	\$13,080.00	\$15,120.00	\$28,200.00
2.	1,080	2,040	S.F.	Standard Concrete Commercial Driveway	\$4.50	\$4,860.00	\$9,180.00	\$14,040.00
3.	10,400	6,090	S.F.	Concrete Walks (incl. Bus Stop Pads)	\$3.50	\$36,400.00	\$21,315.00	\$57,715.00
<b>TOTAL CONCRETE WORK</b>						<b>\$54,340.00</b>	<b>\$45,615.00</b>	<b>\$99,955.00</b>



## STORM SEWER

1.	120	300	L.F.	12" C-900 Storm Lateral (Imp. Backfill)	\$34.00	\$4,080.00	\$10,200.00	\$14,280.00
2.	745		L.F.	24" Storm Main	\$60.00	\$44,700.00		\$44,700.00
3.	5		E.A.	Curb Inlet (Type CG-30)	\$1,100.00	\$5,500.00		\$5,500.00
4.	1		E.A.	Curb Inlet (Type CG-48)	\$1,300.00	\$1,300.00		\$1,300.00
5.	4		E.A.	Ditch Inlet	\$1,200.00	\$4,800.00		\$4,800.00
6.	2		E.A.	48" Conc. Stm. MH	\$2,000.00	\$4,000.00		\$4,000.00
7.	1		E.A.	Core & Connect to Ex 48" Conc. Stm. MH	\$1,500.00	\$1,500.00		\$1,500.00
8.	1,050		L.F.	Trench Protection	\$5.00	\$5,250.00		\$5,250.00
9.	30		C.Y.	Trench Foundation (If Nec.)	\$30.00	\$900.00		\$900.00
10.	50		C.Y.	Rock Excavation (If Necessary)	\$100.00	\$5,000.00		\$5,000.00
TOTAL STORM SEWER						\$77,030.00	\$10,200.00	\$87,230.00

## SANITARY

1.	1,200		L.F.	8 Inch Sanitary Sewer Pipe, 10-ft Depth	\$85.00	\$102,000.00		\$102,000.00
2.	3		E.A.	Sanitary Manhole	\$2,000.00	\$6,000.00		\$6,000.00
3.	1,200		L.F.	Trench Protection	\$4.00	\$4,800.00		\$4,800.00
4.	50		C.Y.	Rock Excavation	\$100.00	\$5,000.00		\$5,000.00
TOTAL SANITARY WORK						\$117,800.00		\$117,800.00

## WATER

1.	1,200		L.F.	18 Inch D.I. Potable Water Pipes and Fittings	\$112.00	\$134,400.00		\$134,400.00
2.			L.F.	8" Inch D.I. Potable Water Pipes and Fittings	\$52.00			
3.	4		E.A.	18" Butterfly Valves	\$2,600.00	\$10,400.00		\$10,400.00
4.			E.A.	8" Gave Valves	\$860.00			
5.	2		E.A.	Hydrant Assemblies	\$2,050.00	\$4,100.00		\$4,100.00
6.	1,200		L.F.	Trench Protection	\$1.50	\$1,800.00		\$1,800.00
TOTAL WATER WORK						\$150,700.00		\$150,700.00

## CONDUIT SYSTEM AND VAULTS

1.		150	L.F.	12" PVC C-900 Conduit Sleeves	\$12.00		\$1,800.00	\$1,800.00
2.	1,135		L.F.	Convert Overhead Utilities to Underground	\$100.00	\$113,500.00		\$113,500.00
3.	20		C.Y.	Trench Foundation (If Nec.)	\$30.00	\$600.00		\$600.00
4.	20		C.Y.	Rock Excavation (If Nec.)	\$100.00	\$2,000.00		\$2,000.00
TOTAL CONDUIT SYSTEM AND VAULTS						\$116,100.00	\$1,800.00	\$117,900.00

## EROSION CONTROL

1.	1,200	1,000	L.F.	Sediment Fence, Unsupported	\$3.00	\$3,600.00	\$3,000.00	\$6,600.00
2.	3	8	EA.	Inlet Protection (Type 3)	\$55.00	\$165.00	\$440.00	\$605.00
3.			L.S.	Permanent Seeding, Complete		\$15,000.00	\$200.00	\$15,200.00
TOTAL EROSION CONTROL						\$18,765.00	\$3,640.00	\$22,405.00

## SIGNING AND STRIPING

1.			L.S.	Perm. Striping And Pavement Marking		\$2,500.00	\$2,500.00	\$5,000.00
2.			L.S.	Permanent Sign Installation, Complete		\$1,300.00	\$1,300.00	\$2,600.00
3.			L.S.	Remove Existing Signs		\$250.00	\$250.00	\$500.00
4.			L.S.	Remove and Reinstall Ex. Signs		\$500.00	\$500.00	\$1,000.00
TOTAL SIGNING AND STRIPING						\$4,550.00	\$4,550.00	\$9,100.00

## STREET LIGHTING

1.			L.S.	Roadway Lighting, Complete		\$24,000.00	\$10,500.00	\$34,500.00
TOTAL STREET LIGHTING						\$24,000.00	\$10,500.00	\$34,500.00

## MISCELLANEOUS CONSTRUCTION

1.		3	EA.	Adjust Existing Gas Valves to F.G.	\$200.00		\$600.00	\$600.00
2.			L.S.	Franchise Utility Markers to be Relocated		\$375.00	\$375.00	\$750.00
3.		1	EA.	Adj. Ex. Telephone MH Rim (Minor Adj.)	\$700.00		\$700.00	\$700.00
4.		1	EA.	Adjust Ex. Utility Vault to F.G.	\$700.00		\$700.00	\$700.00
5.		1	EA.	Centerline Monument	\$300.00		\$300.00	\$300.00
6.	1,200	1,000	L.F.	Orange Safety Fence	\$3.00	\$3,600.00	\$3,000.00	\$6,600.00
TOTAL MISCELLANEOUS CONST.						\$3,975.00	\$5,675.00	\$9,650.00

## LANDSCAPING

1.			L.S.	Landscaping, Complete		\$40,000.00	\$10,000.00	\$50,000.00
TOTAL LANDSCAPING						\$40,000.00	\$10,000.00	\$50,000.00

**TOTAL CONSTRUCTION**

<b>\$937,772.00</b>	<b>\$437,289.60</b>	<b>\$1,375,061.60</b>
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## MISCELLANEOUS

1.	19,800	5,000	S.F.	Right-of-Way	\$14.00	\$277,200.00	\$70,000.00	\$347,200.00
2.	10,900	7,400	S.F.	Temporary Construction Easement	\$1.00	\$10,900.00	\$7,400.00	\$18,300.00
3.				Design & Construction Costs	30%	\$281,331.60	\$131,186.88	\$412,518.48
4.				Contingency	40%	\$375,108.80	\$174,915.84	\$550,024.64
TOTAL MISCELLANEOUS						\$944,540.40	\$383,502.72	\$1,328,043.12

**TOTAL BOONES FERRY ROAD IMPROVEMENTS  
FROM WILSONVILLE ROAD TO BAILEY STREET  
PRELIMINARY COST ESTIMATE**

<b>\$1,882,000.00</b>	<b>\$821,000.00</b>	<b>\$2,703,000.00</b>
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**SUBSECTION B4**

**PROJECT: BOONES FERRY STREET IMPROVEMENTS - PRELIMINARY COST ESTIMATE  
FROM BAILEY STREET TO FIFTH STREET, APPROXIMATELY 680-FEET  
MAJOR COLLECTOR WITH PARKING**

**OWNER: CITY OF WILSONVILLE**

**DATE: APRIL 16, 2008**

ITEM	QUANTITY	UNIT	DESCRIPTION	ENGINEER'S EST.	
				UNIT PRICE	TOTAL

**STREET WORK**

1.		L.S.	Mobilization	\$77,800.00	\$77,800.00
2.		L.S.	Temp. Protection & Direction of Traffic	\$12,200.00	\$12,200.00
3.	1,100	HRS.	Flaggers	\$40.00	\$44,000.00
4.	90	Day	Portable Changeable Message Signs	\$200.00	\$18,000.00
5.		L.S.	Watering	\$4,000.00	\$4,000.00
6.		L.S.	Clearing and Grubbing	\$3,000.00	\$3,000.00
7.	1,544	C.Y.	Unclassified Excavation	\$14.00	\$21,616.00
8.	636	Ton	1/2" Dense Graded Level 3 A.C Pavement	\$85.00	\$54,060.00
9.	547	Ton	3/4" Dense Graded Level 3 A.C Pavement	\$85.00	\$46,495.00
10.	928	C.Y.	Aggregate Base	\$40.00	\$37,120.00
11.	1,360	L.F.	Asphalt Pavement Cutting	\$2.00	\$2,720.00
12.	928	C.Y.	Rock Excavation (If Nec.)	\$35.00	\$32,480.00
13.	428	C.Y.	Import Subgrade Stab. Rock (If Nec.)	\$25.00	\$10,700.00
14.	2,568	S.Y.	Subgrade Geotextile (If Nec.)	\$1.00	\$2,568.00
<b>TOTAL STREET WORK</b>					<b>\$366,800.00</b>

**CONCRETE WORK**

1.	1,360	L.F.	Curb and Gutter, Concrete Curbs	\$12.00	\$16,320.00
2.	350	S.Y.	Standard Concrete Commercial Driveway	\$75.00	\$26,250.00
3.	755	S.Y.	Concrete Walks (incl. Bus Stop Pads)	\$45.00	\$33,975.00
<b>TOTAL CONCRETE WORK</b>					<b>\$76,545.00</b>

**STORM SEWER**

1.	200	L.F.	12" C-900 Storm Lateral (Imp. Backfill)	\$34.00	\$6,800.00
2.	680	L.F.	24" Storm Main	\$60.00	\$40,800.00
3.	2	EA.	Curb Inlet (Type CG-30)	\$1,100.00	\$2,200.00
4.	4	EA.	Curb Inlet (Type CG-48)	\$1,300.00	\$5,200.00
5.		EA.	Ditch Inlet	\$1,200.00	
6.	3	EA.	48" Conc. Stm. MH	\$2,000.00	\$6,000.00
7.	2	EA.	Core & Connect to Ex 48" Conc. Stm. MH	\$1,500.00	\$3,000.00
8.	680	L.F.	Trench Protection	\$5.00	\$3,400.00
9.	126	C.Y.	Trench Foundation (If Nec.)	\$30.00	\$3,780.00
10.	200	C.Y.	Rock Excavation (If Necessary)	\$100.00	\$20,000.00
<b>TOTAL STORM SEWER</b>					<b>\$91,180.00</b>

**SANITARY**

1.	680	L.F.	8 Inch Sanitary Sewer Pipe, 10-ft Depth	\$85.00	\$57,800.00
2.	2	E.A.	Sanitary Manhole	\$2,000.00	\$4,000.00
3.	680	L.F.	Trench Protection	\$4.00	\$2,720.00
4.	30	C.Y.	Rock Excavation	\$100.00	\$3,000.00
<b>TOTAL SANITARY WORK</b>					<b>\$67,520.00</b>

## WATER

1.	680	L.F.	18 Inch D.I. Potable Water Pipes and Fittings	\$112.00	\$76,160.00
2.		L.F.	8" Inch D.I. Potable Water Pipes and Fittings	\$52.00	
3.	2	E.A.	18" Butterfly Valves	\$2,600.00	\$5,200.00
4.		E.A.	8" Gave Valves	\$860.00	
5.	2	E.A.	Hydrant Assemblies	\$2,050.00	\$4,100.00
6.	680	L.F.	Trench Protection	\$1.50	\$1,020.00
TOTAL WATER WORK					\$86,480.00

## CONDUIT SYSTEM AND VAULTS

1.	400	L.F.	12" PVC C-900 Conduit Sleeves	\$12.00	\$4,800.00
2.	680	L.F.	Convert Overhead Utilities to Underground	\$100.00	\$68,000.00
3.	130	C.Y.	Trench Foundation (If Nec.)	\$30.00	\$3,900.00
4.	150	C.Y.	Rock Excavation (If Nec.)	\$100.00	\$15,000.00
TOTAL CONDUIT SYSTEM AND VAULTS					\$91,700.00

## EROSION CONTROL

1.	1,360	L.F.	Sediment Fence, Unsupported	\$3.00	\$4,080.00
2.	6	E.A.	Inlet Protection (Type 3)	\$55.00	\$330.00
3.		L.S.	Permanent Seeding, Complete	\$1,500.00	\$1,500.00
TOTAL EROSION CONTROL					\$5,910.00

## SIGNING AND STRIPING

1.		L.S.	Perm. Striping And Pavement Marking	\$3,500.00	\$3,500.00
2.		L.S.	Permanent Sign Installation, Complete	\$1,800.00	\$1,800.00
3.		L.S.	Remove Existing Signs	\$400.00	\$400.00
4.		L.S.	Remove and Reinstall Ex. Signs	\$700.00	\$700.00
TOTAL SIGNING AND STRIPING					\$6,400.00

## STREET LIGHTING

1.	5	E.A.	Roadway Lighting, Darksky Type	\$4,500.00	\$22,500.00
TOTAL STREET LIGHTING					\$22,500.00

## MISCELLANEOUS CONSTRUCTION

1.	4	E.A.	Adjust Existing Gas Valves to F.G.	\$200.00	\$800.00
2.		L.S.	Franchise Utility Markers to be Relocated	\$550.00	\$550.00
3.	2	E.A.	Centerline Monument	\$300.00	\$600.00
4.	1,360	L.F.	Orange Safety Fence	\$3.00	\$4,080.00
TOTAL MISCELLANEOUS CONST.					\$6,030.00

## LANDSCAPING

1.		L.S.	Landscaping, Complete	\$35,000.00	\$35,000.00
TOTAL LANDSCAPING					\$35,000.00

**TOTAL CONSTRUCTION****\$856,065.00**

## RIGHT-OF-WAY

1.	18,360	S.F.	Right-of-Way	\$14.00	\$257,000.00
2.	3,500	S.F.	Temporary Construction Easement	\$1.00	\$3,500.00
TOTAL RIGHT-OF-WAY					\$260,500.00

## DESIGN, CONSTRUCTION AND CONTINGENCY

1.			Design & Construction Costs	30%	\$256,800.00
2.			Contingency	40%	\$342,400.00
TOTAL MISCELLANEOUS					\$599,200.00

**TOTAL BOONES FERRY ROAD IMPROVEMENTS  
FROM BAILEY STREET TO FIFTH STREET  
PRELIMINARY COST ESTIMATE**

**\$1,716,000.00**

**SUBSECTION B5**

**PROJECT: BOONES FERRY STREET IMPROVEMENTS - PRELIMINARY COST ESTIMATE  
FROM BAILEY STREET TO FIFTH STREET, APPROXIMATELY 680-FEET  
MINOR COLLECTOR WITH PARKING**

**OWNER: CITY OF WILSONVILLE**

**DATE: APRIL 16, 2008**

ITEM	QUANTITY	UNIT	DESCRIPTION	ENGINEER'S EST.	
				UNIT PRICE	TOTAL

**STREET WORK**

1.		L.S.	Mobilization	\$70,300.00	\$70,300.00
2.		L.S.	Temp. Protection & Direction of Traffic	\$10,800.00	\$10,800.00
3.	1,100	HRS.	Flaggers	\$40.00	\$44,000.00
4.	90	Day	Portable Changeable Message Signs	\$200.00	\$18,000.00
5.		L.S.	Watering	\$4,000.00	\$4,000.00
6.		L.S.	Clearing and Grubbing	\$3,000.00	\$3,000.00
7.	986	C.Y.	Unclassified Excavation	\$14.00	\$13,804.00
8.	523	Ton	1/2" Dense Graded Level 3 A.C Pavement	\$85.00	\$44,455.00
9.	322	Ton	3/4" Dense Graded Level 3 A.C Pavement	\$85.00	\$27,370.00
10.	546	C.Y.	Aggregate Base	\$40.00	\$21,840.00
11.	1,360	L.F.	Asphalt Pavement Cutting	\$2.00	\$2,720.00
12.	546	C.Y.	Rock Excavation (If Nec.)	\$35.00	\$19,110.00
13.	252	C.Y.	Import Subgrade Stab. Rock (If Nec.)	\$25.00	\$6,300.00
14.	1,511	S.Y.	Subgrade Geotextile (If Nec.)	\$1.00	\$1,511.00
<b>TOTAL STREET WORK</b>					<b>\$287,200.00</b>

**CONCRETE WORK**

1.	1,360	L.F.	Curb and Gutter, Concrete Curbs	\$12.00	\$16,320.00
2.	350	S.Y.	Standard Concrete Commercial Driveway	\$75.00	\$26,250.00
3.	755	S.Y.	Concrete Walks (incl. Bus Stop Pads)	\$45.00	\$33,975.00
<b>TOTAL CONCRETE WORK</b>					<b>\$76,545.00</b>

**STORM SEWER**

1.	100	L.F.	12" C-900 Storm Lateral (Imp. Backfill)	\$34.00	\$3,400.00
2.	680	L.F.	24" Storm Main	\$60.00	\$40,800.00
3.	2	EA.	Curb Inlet (Type CG-30)	\$1,100.00	\$2,200.00
4.	4	EA.	Curb Inlet (Type CG-48)	\$1,300.00	\$5,200.00
5.		EA.	Ditch Inlet	\$1,200.00	
6.	3	EA.	48" Conc. Stm. MH	\$2,000.00	\$6,000.00
7.	2	EA.	Core & Connect to Ex 48" Conc. Stm. MH	\$1,500.00	\$3,000.00
8.	680	L.F.	Trench Protection	\$5.00	\$3,400.00
9.	126	C.Y.	Trench Foundation (If Nec.)	\$30.00	\$3,780.00
10.	200	C.Y.	Rock Excavation (If Necessary)	\$100.00	\$20,000.00
<b>TOTAL STORM SEWER</b>					<b>\$87,780.00</b>

**SANITARY**

1.	680	L.F.	8 Inch Sanitary Sewer Pipe, 10-ft Depth	\$85.00	\$57,800.00
2.	2	E.A.	Sanitary Manhole	\$2,000.00	\$4,000.00
3.	680	L.F.	Trench Protection	\$4.00	\$2,720.00
4.	30	C.Y.	Rock Excavation	\$100.00	\$3,000.00
<b>TOTAL SANITARY WORK</b>					<b>\$67,520.00</b>

## WATER

1.	680	L.F.	18 Inch D.I. Potable Water Pipes and Fittings	\$112.00	\$76,160.00
2.		L.F.	8" Inch D.I. Potable Water Pipes and Fittings	\$52.00	
3.	2	E.A.	18" Butterfly Valves	\$2,600.00	\$5,200.00
4.		E.A.	8" Gave Valves	\$860.00	
5.	2	E.A.	Hydrant Assemblies	\$2,050.00	\$4,100.00
6.	680	L.F.	Trench Protection	\$1.50	\$1,020.00
TOTAL WATER WORK					\$86,480.00

## CONDUIT SYSTEM AND VAULTS

1.	400	L.F.	12" PVC C-900 Conduit Sleeves	\$12.00	\$4,800.00
2.	680	L.F.	Convert Overhead Utilities to Underground	\$100.00	\$68,000.00
3.	130	C.Y.	Trench Foundation (If Nec.)	\$30.00	\$3,900.00
4.	150	C.Y.	Rock Excavation (If Nec.)	\$100.00	\$15,000.00
TOTAL CONDUIT SYSTEM AND VAULTS					\$91,700.00

## EROSION CONTROL

1.	1,360	L.F.	Sediment Fence, Unsupported	\$3.00	\$4,080.00
2.	6	E.A.	Inlet Protection (Type 3)	\$55.00	\$330.00
3.		L.S.	Permanent Seeding, Complete	\$1,500.00	\$1,500.00
TOTAL EROSION CONTROL					\$5,910.00

## SIGNING AND STRIPING

1.		L.S.	Perm. Striping And Pavement Marking	\$3,500.00	\$3,500.00
2.		L.S.	Permanent Sign Installation, Complete	\$1,800.00	\$1,800.00
3.		L.S.	Remove Existing Signs	\$400.00	\$400.00
4.		L.S.	Remove and Reinstall Ex. Signs	\$700.00	\$700.00
TOTAL SIGNING AND STRIPING					\$6,400.00

## STREET LIGHTING

1.	5	E.A.	Roadway Lighting, Darksky Type	\$4,500.00	\$22,500.00
TOTAL STREET LIGHTING					\$22,500.00

## MISCELLANEOUS CONSTRUCTION

1.	4	E.A.	Adjust Existing Gas Valves to F.G.	\$200.00	\$800.00
2.		L.S.	Franchise Utility Markers to be Relocated	\$550.00	\$550.00
3.	2	E.A.	Centerline Monument	\$300.00	\$600.00
4.	1,360	L.F.	Orange Safety Fence	\$3.00	\$4,080.00
TOTAL MISCELLANEOUS CONST.					\$6,030.00

## LANDSCAPING

1.		L.S.	Landscaping, Complete	\$35,000.00	\$35,000.00
TOTAL LANDSCAPING					\$35,000.00

**TOTAL CONSTRUCTION****\$773,065.00**

## RIGHT-OF-WAY

1.	8,840	S.F.	Right-of-Way	\$14.00	\$123,800.00
2.	3,500	S.F.	Temporary Construction Easement	\$1.00	\$3,500.00
TOTAL RIGHT-OF-WAY					\$127,300.00

## DESIGN, CONSTRUCTION AND CONTINGENCY

1.			Design & Construction Costs	30%	\$231,900.00
2.			Contingency	40%	\$309,200.00
TOTAL MISCELLANEOUS					\$541,100.00

**TOTAL BOONES FERRY ROAD IMPROVEMENTS  
FROM BAILEY STREET TO FIFTH STREET  
PRELIMINARY COST ESTIMATE**
**\$1,441,000.00**



**SUBSECTION B6**

**PROJECT: BOONES FERRY STREET IMPROVEMENTS - PRELIMINARY COST ESTIMATE  
FROM FIFTH STREET TO FOURTH STREET, APPROXIMATELY 415-FEET  
MAJOR COLLECTOR PARKING**

**OWNER: CITY OF WILSONVILLE**

**DATE: APRIL 16, 2008**

ITEM	QUANTITY	UNIT	DESCRIPTION	ENGINEER'S EST.	
				UNIT PRICE	TOTAL

**STREET WORK**

1.		L.S.	Mobilization	\$49,400.00	\$49,400.00
2.		L.S.	Temp. Protection & Direction of Traffic	\$7,800.00	\$7,800.00
3.	1,100	HRS.	Flaggers	\$40.00	\$44,000.00
4.	90	Day	Portable Changeable Message Signs	\$200.00	\$18,000.00
5.		L.S.	Watering	\$2,500.00	\$2,500.00
6.		L.S.	Clearing and Grubbing	\$3,000.00	\$3,000.00
7.	945	C.Y.	Unclassified Excavation	\$14.00	\$13,230.00
8.	388	Ton	1/2" Dense Graded Level 3 A.C Pavement	\$85.00	\$32,980.00
9.	334	Ton	3/4" Dense Graded Level 3 A.C Pavement	\$85.00	\$28,390.00
10.	566	C.Y.	Aggregate Base	\$40.00	\$22,640.00
11.	830	L.F.	Asphalt Pavement Cutting	\$2.00	\$1,660.00
12.	566	C.Y.	Rock Excavation (If Nec.)	\$35.00	\$19,810.00
13.	261	C.Y.	Import Subgrade Stab. Rock (If Nec.)	\$25.00	\$6,525.00
14.	1,568	S.Y.	Subgrade Geotextile (If Nec.)	\$1.00	\$1,568.00
<b>TOTAL STREET WORK</b>					<b>\$251,500.00</b>

**CONCRETE WORK**

1.	830	L.F.	Curb and Gutter, Concrete Curbs	\$12.00	\$9,960.00
2.	100	S.Y.	Standard Concrete Commercial Driveway	\$75.00	\$7,500.00
3.	461	S.Y.	Concrete Walks (incl. Bus Stop Pads)	\$45.00	\$20,745.00
<b>TOTAL CONCRETE WORK</b>					<b>\$38,205.00</b>

**STORM SEWER**

1.	128	L.F.	12" C-900 Storm Lateral (Imp. Backfill)	\$34.00	\$4,352.00
2.	415	L.F.	24" Storm Main	\$60.00	\$24,900.00
3.		EA.	Curb Inlet (Type CG-30)	\$1,100.00	
4.	4	EA.	Curb Inlet (Type CG-48)	\$1,300.00	\$5,200.00
5.		EA.	Ditch Inlet	\$1,200.00	
6.	2	EA.	48" Conc. Stm. MH	\$2,000.00	\$4,000.00
7.	2	EA.	Core & Connect to Ex 48" Conc. Stm. MH	\$1,500.00	\$3,000.00
8.	415	L.F.	Trench Protection	\$5.00	\$2,075.00
9.	76	C.Y.	Trench Foundation (If Nec.)	\$30.00	\$2,280.00
10.	123	C.Y.	Rock Excavation (If Necessary)	\$100.00	\$12,300.00
<b>TOTAL STORM SEWER</b>					<b>\$58,107.00</b>

**SANITARY**

1.	415	L.F.	8 Inch Sanitary Sewer Pipe, 10-ft Depth	\$85.00	\$35,275.00
2.	2	E.A.	Sanitary Manhole	\$2,000.00	\$4,000.00
3.	415	L.F.	Trench Protection	\$4.00	\$1,660.00
4.	20	C.Y.	Rock Excavation	\$100.00	\$2,000.00
<b>TOTAL SANITARY WORK</b>					<b>\$42,935.00</b>

## WATER

1.	415	L.F.	18 Inch D.I. Potable Water Pipes and Fittings	\$112.00	\$46,480.00
2.		L.F.	8" Inch D.I. Potable Water Pipes and Fittings	\$52.00	
3.	2	E.A.	18" Butterfly Valves	\$2,600.00	\$5,200.00
4.		E.A.	8" Gave Valves	\$860.00	
5.	1	E.A.	Hydrant Assemblies	\$2,050.00	\$2,050.00
6.	415	L.F.	Trench Protection	\$1.50	\$622.50
TOTAL WATER WORK					\$54,352.50

## CONDUIT SYSTEM AND VAULTS

1.	400	L.F.	12" PVC C-900 Conduit Sleeves	\$12.00	\$4,800.00
2.	415	L.F.	Convert Overhead Utilities to Underground	\$100.00	\$41,500.00
3.	100	C.Y.	Trench Foundation (If Nec.)	\$30.00	\$3,000.00
4.	100	C.Y.	Rock Excavation (If Nec.)	\$100.00	\$10,000.00
TOTAL CONDUIT SYSTEM AND VAULTS					\$59,300.00

## EROSION CONTROL

1.	830	L.F.	Sediment Fence, Unsupported	\$3.00	\$2,490.00
2.	4	E.A.	Inlet Protection (Type 3)	\$55.00	\$220.00
3.		L.S.	Permanent Seeding, Complete	\$1,000.00	\$1,000.00
TOTAL EROSION CONTROL					\$3,710.00

## SIGNING AND STRIPING

1.		L.S.	Perm. Striping And Pavement Marking	\$1,500.00	\$1,500.00
2.		L.S.	Permanent Sign Installation, Complete	\$800.00	\$800.00
3.		L.S.	Remove Existing Signs	\$200.00	\$200.00
4.		L.S.	Remove and Reinstall Ex. Signs	\$300.00	\$300.00
TOTAL SIGNING AND STRIPING					\$2,800.00

## STREET LIGHTING

1.	3	E.A.	Roadway Lighting, Darksky Type	\$4,500.00	\$13,500.00
TOTAL STREET LIGHTING					\$13,500.00

## MISCELLANEOUS CONSTRUCTION

1.	2	E.A.	Adjust Existing Gas Valves to F.G.	\$200.00	\$400.00
2.		L.S.	Franchise Utility Markers to be Relocated	\$550.00	\$550.00
3.	2	E.A.	Centerline Monument	\$300.00	\$600.00
4.	830	L.F.	Orange Safety Fence	\$3.00	\$2,490.00
TOTAL MISCELLANEOUS CONST.					\$4,040.00

## LANDSCAPING

1.		L.S.	Landscaping, Complete	\$15,000.00	\$15,000.00
TOTAL LANDSCAPING					\$15,000.00

**TOTAL CONSTRUCTION****\$543,449.50**

## RIGHT-OF-WAY

1.	11,205	S.F.	Right-of-Way	\$14.00	\$156,900.00
2.	1,000	S.F.	Temporary Construction Easement	\$1.00	\$1,000.00
TOTAL RIGHT-OF-WAY					\$157,900.00

## DESIGN, CONSTRUCTION AND CONTINGENCY

1.			Design & Construction Costs	30%	\$163,000.00
2.			Contingency	40%	\$217,400.00
TOTAL MISCELLANEOUS					\$380,400.00

**TOTAL BOONES FERRY ROAD IMPROVEMENTS  
FROM FIFTH STREET TO FOURTH STREET  
PRELIMINARY COST ESTIMATE**
**\$1,082,000.00**

**SUBSECTION B7**

**PROJECT: BOONES FERRY STREET IMPROVEMENTS - PRELIMINARY COST ESTIMATE  
FROM FIFTH STREET TO FOURTH STREET, APPROXIMATELY 415-FEET  
MINOR COLLECTOR WITH PARKING**

**OWNER: CITY OF WILSONVILLE**

**DATE: APRIL 16, 2008**

ITEM	QUANTITY	UNIT	DESCRIPTION	ENGINEER'S EST.	
				UNIT PRICE	TOTAL

**STREET WORK**

1.		L.S.	Mobilization	\$44,900.00	\$44,900.00
2.		L.S.	Temp. Protection & Direction of Traffic	\$6,900.00	\$6,900.00
3.	1,100	HRS.	Flaggers	\$40.00	\$44,000.00
4.	90	Day	Portable Changeable Message Signs	\$200.00	\$18,000.00
5.		L.S.	Watering	\$2,500.00	\$2,500.00
6.		L.S.	Clearing and Grubbing	\$3,000.00	\$3,000.00
7.	602	C.Y.	Unclassified Excavation	\$14.00	\$8,428.00
8.	319	Ton	1/2" Dense Graded Level 3 A.C Pavement	\$85.00	\$27,115.00
9.	196	Ton	3/4" Dense Graded Level 3 A.C Pavement	\$85.00	\$16,660.00
10.	333	C.Y.	Aggregate Base	\$40.00	\$13,320.00
11.	830	L.F.	Asphalt Pavement Cutting	\$2.00	\$1,660.00
12.	333	C.Y.	Rock Excavation (If Nec.)	\$35.00	\$11,655.00
13.	154	C.Y.	Import Subgrade Stab. Rock (If Nec.)	\$25.00	\$3,850.00
14.	922	S.Y.	Subgrade Geotextile (If Nec.)	\$1.00	\$922.00
<b>TOTAL STREET WORK</b>					<b>\$202,900.00</b>

**CONCRETE WORK**

1.	830	L.F.	Curb and Gutter, Concrete Curbs	\$12.00	\$9,960.00
2.	100	S.Y.	Standard Concrete Commercial Driveway	\$75.00	\$7,500.00
3.	461	S.Y.	Concrete Walks (incl. Bus Stop Pads)	\$45.00	\$20,745.00
<b>TOTAL CONCRETE WORK</b>					<b>\$38,205.00</b>

**STORM SEWER**

1.	100	L.F.	12" C-900 Storm Lateral (Imp. Backfill)	\$34.00	\$3,400.00
2.	415	L.F.	24" Storm Main	\$60.00	\$24,900.00
3.		EA.	Curb Inlet (Type CG-30)	\$1,100.00	
4.	4	EA.	Curb Inlet (Type CG-48)	\$1,300.00	\$5,200.00
5.		EA.	Ditch Inlet	\$1,200.00	
6.	2	EA.	48" Conc. Stm. MH	\$2,000.00	\$4,000.00
7.	2	EA.	Core & Connect to Ex 48" Conc. Stm. MH	\$1,500.00	\$3,000.00
8.	415	L.F.	Trench Protection	\$5.00	\$2,075.00
9.	76	C.Y.	Trench Foundation (If Nec.)	\$30.00	\$2,280.00
10.	123	C.Y.	Rock Excavation (If Necessary)	\$100.00	\$12,300.00
<b>TOTAL STORM SEWER</b>					<b>\$57,155.00</b>

**SANITARY**

1.	415	L.F.	8 Inch Sanitary Sewer Pipe, 10-ft Depth	\$85.00	\$35,275.00
2.	2	E.A.	Sanitary Manhole	\$2,000.00	\$4,000.00
3.	415	L.F.	Trench Protection	\$4.00	\$1,660.00
4.	20	C.Y.	Rock Excavation	\$100.00	\$2,000.00
<b>TOTAL SANITARY WORK</b>					<b>\$42,935.00</b>

## WATER

1.	415	L.F.	18 Inch D.I. Potable Water Pipes and Fittings	\$112.00	\$46,480.00
2.		L.F.	8" Inch D.I. Potable Water Pipes and Fittings	\$52.00	
3.	2	E.A.	18" Butterfly Valves	\$2,600.00	\$5,200.00
4.		E.A.	8" Gave Valves	\$860.00	
5.	1	E.A.	Hydrant Assemblies	\$2,050.00	\$2,050.00
6.	415	L.F.	Trench Protection	\$1.50	\$622.50
TOTAL WATER WORK					\$54,352.50

## CONDUIT SYSTEM AND VAULTS

1.	400	L.F.	12" PVC C-900 Conduit Sleeves	\$12.00	\$4,800.00
2.	415	L.F.	Convert Overhead Utilities to Underground	\$100.00	\$41,500.00
3.	100	C.Y.	Trench Foundation (If Nec.)	\$30.00	\$3,000.00
4.	100	C.Y.	Rock Excavation (If Nec.)	\$100.00	\$10,000.00
TOTAL CONDUIT SYSTEM AND VAULTS					\$59,300.00

## EROSION CONTROL

1.	830	L.F.	Sediment Fence, Unsupported	\$3.00	\$2,490.00
2.	4	E.A.	Inlet Protection (Type 3)	\$55.00	\$220.00
3.		L.S.	Permanent Seeding, Complete	\$1,000.00	\$1,000.00
TOTAL EROSION CONTROL					\$3,710.00

## SIGNING AND STRIPING

1.		L.S.	Perm. Striping And Pavement Marking	\$1,500.00	\$1,500.00
2.		L.S.	Permanent Sign Installation, Complete	\$800.00	\$800.00
3.		L.S.	Remove Existing Signs	\$200.00	\$200.00
4.		L.S.	Remove and Reinstall Ex. Signs	\$300.00	\$300.00
TOTAL SIGNING AND STRIPING					\$2,800.00

## STREET LIGHTING

1.	3	E.A.	Roadway Lighting, Darksky Type	\$4,500.00	\$13,500.00
TOTAL STREET LIGHTING					\$13,500.00

## MISCELLANEOUS CONSTRUCTION

1.	2	E.A.	Adjust Existing Gas Valves to F.G.	\$200.00	\$400.00
2.		L.S.	Franchise Utility Markers to be Relocated	\$550.00	\$550.00
3.	2	E.A.	Centerline Monument	\$300.00	\$600.00
4.	830	L.F.	Orange Safety Fence	\$3.00	\$2,490.00
TOTAL MISCELLANEOUS CONST.					\$4,040.00

## LANDSCAPING

1.		L.S.	Landscaping, Complete	\$15,000.00	\$15,000.00
TOTAL LANDSCAPING					\$15,000.00

**TOTAL CONSTRUCTION****\$493,897.50**

## RIGHT-OF-WAY

1.	5,395	S.F.	Right-of-Way	\$14.00	\$75,500.00
2.	1,000	S.F.	Temporary Construction Easement	\$1.00	\$1,000.00
TOTAL RIGHT-OF-WAY					\$76,500.00

## DESIGN, CONSTRUCTION AND CONTINGENCY

1.			Design & Construction Costs	30%	\$148,200.00
2.			Contingency	40%	\$197,600.00
TOTAL MISCELLANEOUS					\$345,800.00

**TOTAL BOONES FERRY ROAD IMPROVEMENTS  
FROM FIFTH STREET TO FOURTH STREET  
PRELIMINARY COST ESTIMATE**

**\$916,000.00**

**SUBSECTION B8**

**PROJECT:** BOONES FERRY STREET IMPROVEMENTS - PRELIMINARY COST ESTIMATE  
FROM FOURTH STREET TO SECOND STREET, APPROXIMATELY 590-FEET  
RESIDENTIAL STREET STANDARDS WITH PARKING

**OWNER:** CITY OF WILSONVILLE

**DATE:** APRIL 16, 2008

ITEM	QUANTITY	UNIT	DESCRIPTION	ENGINEER'S EST.	
				UNIT PRICE	TOTAL

**STREET WORK**

1.		L.S.	Mobilization	\$49,700.00	\$49,700.00
2.		L.S.	Temp. Protection & Direction of Traffic	\$7,100.00	\$7,100.00
3.	1,100	HRS.	Flaggers	\$40.00	\$44,000.00
4.	90	Day	Portable Changeable Message Signs	\$200.00	\$18,000.00
5.		L.S.	Watering	\$3,000.00	\$3,000.00
6.		L.S.	Clearing and Grubbing	\$3,000.00	\$3,000.00
7.	219	C.Y.	Unclassified Excavation	\$14.00	\$3,066.00
8.	419	Ton	1/2" Dense Graded Level 3 A.C Pavement	\$85.00	\$35,615.00
9.	100	L.F.	Asphalt Pavement Cutting	\$2.00	\$200.00
<b>TOTAL STREET WORK</b>					<b>\$163,700.00</b>

**CONCRETE WORK**

1.	500	S.Y.	Standard Concrete Residential Driveway	\$40.00	\$20,000.00
2.	656	S.Y.	Concrete Walks (incl. Bus Stop Pads)	\$45.00	\$29,520.00
<b>TOTAL CONCRETE WORK</b>					<b>\$49,520.00</b>

**STORM SEWER**

1.	80	L.F.	12" C-900 Storm Lateral (Imp. Backfill)	\$34.00	\$2,720.00
2.	590	L.F.	24" Storm Main	\$60.00	\$35,400.00
3.	4	EA.	Ditch Inlet	\$1,200.00	\$4,800.00
4.	2	EA.	48" Conc. Strm. MH	\$2,000.00	\$4,000.00
5.	1	EA.	Core & Connect to Ex 48" Conc. Strm. MH	\$1,500.00	\$1,500.00
6.	590	L.F.	Trench Protection	\$5.00	\$2,950.00
7.	109	C.Y.	Trench Foundation (If Nec.)	\$30.00	\$3,270.00
8.	175	C.Y.	Rock Excavation (If Necessary)	\$100.00	\$17,500.00
<b>TOTAL STORM SEWER</b>					<b>\$72,140.00</b>

**SANITARY**

1.	590	L.F.	8 Inch Sanitary Sewer Pipe, 10-ft Depth	\$85.00	\$50,150.00
2.	2	E.A.	Sanitary Manhole	\$2,000.00	\$4,000.00
3.	590	L.F.	Trench Protection	\$4.00	\$2,360.00
4.	25	C.Y.	Rock Excavation	\$100.00	\$2,500.00
<b>TOTAL SANITARY WORK</b>					<b>\$59,010.00</b>

## WATER

1.	590	L.F.	18 Inch D.I. Potable Water Pipes and Fittings	\$112.00	\$66,080.00
2.		L.F.	8" Inch D.I. Potable Water Pipes and Fittings	\$52.00	
3.	2	E.A.	18" Butterfly Valves	\$2,600.00	\$5,200.00
4.		E.A.	8" Gave Valves	\$860.00	
5.	2	E.A.	Hydrant Assemblies	\$2,050.00	\$4,100.00
6.	590	L.F.	Trench Protection	\$1.50	\$885.00
TOTAL WATER WORK					\$76,265.00

## CONDUIT SYSTEM AND VAULTS

1.	200	L.F.	12" PVC C-900 Conduit Sleeves	\$12.00	\$2,400.00
2.	590	L.F.	Convert Overhead Utilities to Underground	\$100.00	\$59,000.00
3.	50	C.Y.	Trench Foundation (If Nec.)	\$30.00	\$1,500.00
4.	50	C.Y.	Rock Excavation (If Nec.)	\$100.00	\$5,000.00
TOTAL CONDUIT SYSTEM AND VAULTS					\$67,900.00

## EROSION CONTROL

1.	1,180	L.F.	Sediment Fence, Unsupported	\$3.00	\$3,540.00
2.	4	E.A.	Inlet Protection (Type 3)	\$55.00	\$220.00
3.	1,000	L.S.	Permanent Seeding, Complete	\$1,500.00	\$1,500.00
TOTAL EROSION CONTROL					\$5,260.00

## SIGNING AND STRIPING

1.		L.S.	Perm. Striping And Pavement Marking	\$2,500.00	\$2,500.00
2.		L.S.	Permanent Sign Installation, Complete	\$1,300.00	\$1,300.00
3.		L.S.	Remove Existing Signs	\$250.00	\$250.00
4.		L.S.	Remove and Reinstall Ex. Signs	\$500.00	\$500.00
TOTAL SIGNING AND STRIPING					\$4,550.00

## STREET LIGHTING

1.	4	E.A.	Roadway Lighting, Darksky Type	\$4,500.00	\$18,000.00
TOTAL STREET LIGHTING					\$18,000.00

## MISCELLANEOUS CONSTRUCTION

1.	2	E.A.	Adjust Existing Gas Valves to F.G.	\$200.00	\$400.00
2.		L.S.	Franchise Utility Markers to be Relocated	\$550.00	\$550.00
3.	3	E.A.	Centerline Monument	\$300.00	\$900.00
4.	1,180	L.F.	Orange Safety Fence	\$3.00	\$3,540.00
TOTAL MISCELLANEOUS CONST.					\$5,390.00



## LANDSCAPING

1.		L.S.	Landscaping, Complete	\$25,000.00	\$25,000.00
TOTAL LANDSCAPING					\$25,000.00

**TOTAL CONSTRUCTION****\$546,735.00**

## RIGHT-OF-WAY

1.	3,000	S.F.	Temporary Construction Easement	\$1.00	\$3,000.00
TOTAL RIGHT-OF-WAY					\$3,000.00

## DESIGN, CONSTRUCTION AND CONTINGENCY

1.			Design & Construction Costs	30%	\$164,000.00
2.			Contingency	40%	\$218,700.00
TOTAL MISCELLANEOUS					\$382,700.00

**TOTAL BOONES FERRY ROAD IMPROVEMENTS  
FROM FOURTH STREET TO SECOND STREET  
PRELIMINARY COST ESTIMATE**

**\$932,000.00**

**SUBSECTION B9**

**PROJECT: BOONES FERRY STREET IMPROVEMENTS - PRELIMINARY COST ESTIMATE  
FROM SECOND STREET TO BRIDGE, APPROXIMATELY 550-FEET  
RESIDENTIAL STREET STANDARDS WITH PARKING**

**OWNER: CITY OF WILSONVILLE**

**DATE: APRIL 16, 2008**

ITEM	QUANTITY	UNIT	DESCRIPTION	ENGINEER'S EST.	
				UNIT PRICE	TOTAL

**STREET WORK**

1.		L.S.	Mobilization	\$38,900.00	\$38,900.00
2.		L.S.	Temp. Protection & Direction of Traffic	\$5,800.00	\$5,800.00
3.	1,100	HRS.	Flaggers	\$40.00	\$44,000.00
4.	90	Day	Portable Changeable Message Signs	\$200.00	\$18,000.00
5.		L.S.	Watering	\$2,500.00	\$2,500.00
6.		L.S.	Clearing and Grubbing	\$3,000.00	\$3,000.00
7.	200	C.Y.	Unclassified Excavation	\$14.00	\$2,800.00
8.	390	Ton	1/2" Dense Graded Level 3 A.C Pavement	\$85.00	\$33,150.00
11.	100	L.F.	Asphalt Pavement Cutting	\$2.00	\$200.00
<b>TOTAL STREET WORK</b>					<b>\$148,400.00</b>

**CONCRETE WORK**

2.	100	S.Y.	Standard Concrete Residential Driveway	\$40.00	\$4,000.00
3.	611	S.Y.	Concrete Walks (incl. Bus Stop Pads)	\$45.00	\$27,495.00
<b>TOTAL CONCRETE WORK</b>					<b>\$31,495.00</b>

**STORM SEWER**

1.	80	L.F.	12" C-900 Storm Lateral (Imp. Backfill)	\$34.00	\$2,720.00
2.	460	L.F.	24" Storm Main	\$60.00	\$27,600.00
5.	6	EA.	Ditch Inlet	\$1,200.00	\$7,200.00
6.	4	EA.	48" Conc. Strm. MH	\$2,000.00	\$8,000.00
7.	2	EA.	Core & Connect to Ex 48" Conc. Strm. MH	\$1,500.00	\$3,000.00
8.	460	L.F.	Trench Protection	\$5.00	\$2,300.00
9.	100	C.Y.	Trench Foundation (If Nec.)	\$30.00	\$3,000.00
10.	165	C.Y.	Rock Excavation (If Necessary)	\$100.00	\$16,500.00
<b>TOTAL STORM SEWER</b>					<b>\$70,320.00</b>

**SANITARY**

1.	550	L.F.	8 Inch Sanitary Sewer Pipe, 10-ft Depth	\$85.00	\$46,750.00
2.	2	E.A.	Sanitary Manhole	\$2,000.00	\$4,000.00
3.	550	L.F.	Trench Protection	\$4.00	\$2,200.00
4.	25	C.Y.	Rock Excavation	\$100.00	\$2,500.00
<b>TOTAL SANITARY WORK</b>					<b>\$55,450.00</b>

## WATER

1.		L.F.	18 Inch D.I. Potable Water Pipes and Fittings	\$112.00		
2.	550	L.F.	8" Inch D.I. Potable Water Pipes and Fittings	\$52.00	\$28,600.00	
3.		E.A.	18" Butterfly Valves	\$2,600.00		
4.	2	E.A.	8" Gave Valves	\$860.00	\$1,720.00	
5.	2	E.A.	Hydrant Assemblies	\$2,050.00	\$4,100.00	
6.	550	L.F.	Trench Protection	\$1.50	\$825.00	
TOTAL WATER WORK						\$35,245.00

## CONDUIT SYSTEM AND VAULTS

1.	100	L.F.	12" PVC C-900 Conduit Sleeves	\$12.00	\$1,200.00	
2.	270	L.F.	Convert Overhead Utilities to Underground	\$100.00	\$27,000.00	
3.	100	C.Y.	Trench Foundation (If Nec.)	\$30.00	\$3,000.00	
4.	100	C.Y.	Rock Excavation (If Nec.)	\$100.00	\$10,000.00	
TOTAL CONDUIT SYSTEM AND VAULTS						\$41,200.00

## EROSION CONTROL

1.	920	L.F.	Sediment Fence, Unsupported	\$3.00	\$2,760.00	
2.	6	E.A.	Inlet Protection (Type 3)	\$55.00	\$330.00	
3.		L.S.	Permanent Seeding, Complete	\$1,000.00	\$1,000.00	
TOTAL EROSION CONTROL						\$4,090.00

## SIGNING AND STRIPING

1.		L.S.	Perm. Striping And Pavement Marking	\$2,000.00	\$2,000.00	
2.		L.S.	Permanent Sign Installation, Complete	\$1,100.00	\$1,100.00	
3.		L.S.	Remove Existing Signs	\$200.00	\$200.00	
4.		L.S.	Remove and Reinstall Ex. Signs	\$400.00	\$400.00	
TOTAL SIGNING AND STRIPING						\$3,700.00

## STREET LIGHTING

1.	3	E.A.	Roadway Lighting, Darksky Type	\$4,500.00	\$13,500.00	
TOTAL STREET LIGHTING						\$13,500.00

## MISCELLANEOUS CONSTRUCTION

1.	2	E.A.	Adjust Existing Gas Valves to F.G.	\$200.00	\$400.00	
2.		L.S.	Franchise Utility Markers to be Relocated	\$550.00	\$550.00	
3.	2	E.A.	Centerline Monument	\$300.00	\$600.00	
4.	920	L.F.	Orange Safety Fence	\$3.00	\$2,760.00	
TOTAL MISCELLANEOUS CONST.						\$4,310.00

## LANDSCAPING

1.		L.S.	Landscaping, Complete	\$20,000.00	\$20,000.00
TOTAL LANDSCAPING					\$20,000.00

**TOTAL CONSTRUCTION****\$427,710.00**

## RIGHT-OF-WAY

2.	1,000	S.F.	Temporary Construction Easement	\$1.00	\$1,000.00
TOTAL RIGHT-OF-WAY					\$1,000.00

## DESIGN, CONSTRUCTION AND CONTINGENCY

1.			Design & Construction Costs	30%	\$128,300.00
2.			Contingency	40%	\$171,100.00
TOTAL MISCELLANEOUS					\$299,400.00

**TOTAL BOONES FERRY ROAD IMPROVEMENTS  
FROM SECOND STREET TO BRIDGE  
PRELIMINARY COST ESTIMATE**

**\$728,000.00**



# **APPENDIX D**

## **Proposed Capital Projects Preliminary Cost Estimates June 2008**

### **Subsection C: Old Town Residential Streetscape Improvements**





MacKay & Sposito, Inc.

**OLD TOWN PRELIMINARY COST ESTIMATES**  
**OLD TOWN**  
**RESIDENTIAL STREETS**

M&S PROJECT #:	14892
DRAWN BY:	EH/MW
CHECKED BY:	RP
DATE:	04/22/08
SHEET NO.:	C1

Scale: 1" = 500'



**SUBSECTION C2**

**PROJECT: RESIDENTIAL STREETScape IMPROVEMENTS - PRELIMINARY COST ESTIMATE**

**RESIDENTIAL STREET STANDARDS WITH GRAVEL PARKING**

**OWNER: CITY OF WILSONVILLE**

**CASE "A"**

**DATE: APRIL 21, 2008**

			ENGINEER'S EST.					
			RESIDENTIAL ROAD DESCRIPTION, BASE ON CASE "A"					
ITEM, BASE ON CASE A1 OR A2 AS NOTED BELOW	TOTAL COST BASED ON 400- FEET ROADWAY, REFER TO PRELIMINARY BASE COST FOR CASE "A"	UNIT PRICE PER LINEAL FOOT OF ROADWAY	MAGNOLIA AVENUE, FROM CUL-DE-SAC TO 280- FEET SOUTH OF 4TH STREET	FIR STREET, FROM 5TH ST. TO 4TH ST.	FIFTH STREET, FROM BOONES FERRY RD TO DEAD END EAST	FOURTH STREET, FROM BOONES FERRY RD TO MAGNOLIA	FOURTH STREET, EAST OF FIR ST.	TAUCHMAN STREET, FROM BOONES FERRY TO ROAD FORKS
ROADWAY LENGTH, FEET			1075	470	920	300	245	730
STREET WORK, CASE A1	\$107,800.00	\$269.50			\$247,940.00			
STREET WORK, CASE A2	\$108,700.00	\$271.75	\$292,131.25	\$127,722.50		\$81,525.00	\$66,578.75	\$198,377.50
CONCRETE WORK, CASE A1	\$30,780.00	\$76.95			\$70,794.00			
CONCRETE WORK, CASE A2	\$30,780.00	\$76.95	\$82,721.25	\$36,166.50		\$23,085.00	\$18,852.75	\$56,173.50
STORM SEWER, CASE A1	\$46,160.00	\$115.40			\$106,168.00			
STORM SEWER, CASE A2	\$46,160.00	\$115.40	\$124,055.00	\$54,238.00		\$34,620.00	\$28,273.00	\$84,242.00
SANITARY SEWER, CASE A1	\$42,100.00	\$105.25			\$96,830.00			
SANITARY SEWER, CASE A2	\$42,100.00	\$105.25	\$113,143.75	\$49,467.50		\$31,575.00	\$25,786.25	\$76,832.50
WATER, CASE A1	\$54,700.00	\$136.75			\$125,810.00			
WATER, CASE A2	\$27,220.00	\$68.05	\$73,153.75	\$31,983.50		\$20,415.00	\$16,672.25	\$49,676.50
CONDUIT SYSTEM AND VAULTS, A1	\$3,700.00	\$9.25			\$8,510.00			
CONDUIT SYSTEM AND VAULTS, A2	\$3,700.00	\$9.25	\$9,943.75	\$4,347.50		\$2,775.00	\$2,266.25	\$6,752.50
EROSION CONTROL, CASE A1	\$2,930.00	\$7.33			\$6,739.00			
EROSION CONTROL, CASE A2	\$2,930.00	\$7.33	\$7,874.38	\$3,442.75		\$2,197.50	\$1,794.63	\$5,347.25
SIGNING AND STRIPING, CASE A1	\$700.00	\$1.75			\$1,610.00			
SIGNING AND STRIPING, CASE A2	\$700.00	\$1.75	\$1,881.25	\$822.50		\$525.00	\$428.75	\$1,277.50
STREET LIGHTING, ASSUMED 1 EACH PER EVERY 150- FEET	\$4,500.00		\$32,250.00	\$14,100.00	\$27,600.00	\$9,000.00	\$7,350.00	\$21,900.00
MISC. CONSTRUCTION, CASE A1	\$1,550.00	\$3.88			\$3,565.00			
MISC. CONSTRUCTION, CASE A2	\$1,550.00	\$3.88	\$4,165.63	\$1,821.25		\$1,162.50	\$949.38	\$2,828.75
LANDSCAPING, CASE A1	\$10,000.00	\$25.00			\$23,000.00			
LANDSCAPING, CASE A2	\$10,000.00	\$25.00	\$26,875.00	\$11,750.00		\$7,500.00	\$6,125.00	\$18,250.00
<b>TOTAL CONSTRUCTION</b>			<b>\$768,195.00</b>	<b>\$335,862.00</b>	<b>\$718,566.00</b>	<b>\$214,380.00</b>	<b>\$175,077.00</b>	<b>\$521,658.00</b>
RIGHT-OF-WAY, PER S.F.		\$14.00					\$285,600.00	
DESIGN AND CONSTRUCTION COST		30.0%	\$230,458.50	\$100,758.60	\$215,569.80	\$64,314.00	\$52,523.10	\$156,497.40
CONTINGENCY		40.0%	\$307,278.00	\$134,344.80	\$287,426.40	\$85,752.00	\$70,030.80	\$208,663.20
<b>TOTAL WITH DESIGN, CONSTRUCTION, AND CONTINGENCY</b>			<b>\$1,306,000.00</b>	<b>\$571,000.00</b>	<b>\$1,222,000.00</b>	<b>\$364,000.00</b>	<b>\$583,000.00</b>	<b>\$887,000.00</b>

**SUBSECTION C3****PROJECT:****RESIDENTIAL STREETScape IMPROVEMENTS - PRELIMINARY COST ESTIMATE****OWNER:****RESIDENTIAL STREET STANDARDS WITH GRAVEL PARKING****CASE "B"****CITY OF WILSONVILLE****DATE: APRIL 21, 2008**

			<b>ENGINEER'S EST.</b>		
			<b>RESIDENTIAL ROAD DESCRIPTION, BASE ON CASE "B"</b>		
<b>ITEM</b>	<b>TOTAL COST BASED ON 400- FEET ROADWAY, REFER TO PRELIMINARY BASE COST FOR CASE "B"</b>	<b>UNIT PRICE PER LINEAL FOOT OF ROADWAY</b>	<b>MAGNOLIA AVENUE, FROM 280-FT SOUTH OF 4TH TO TAUCHMAN ST.</b>	<b>FIR STREET, SOUTH OF 4TH TO DEAD END</b>	
<b>ROADWAY LENGTH, FEET</b>			<b>615</b>	<b>207</b>	
STREET WORK	\$115,000.00	\$287.50	\$176,812.50	\$59,512.50	
CONCRETE WORK	\$30,780.00	\$76.95	\$47,324.25	\$15,928.65	
STORM SEWER	\$46,160.00	\$115.40	\$70,971.00	\$23,887.80	
SANITARY SEWER	\$42,100.00	\$105.25	\$64,728.75	\$21,786.75	
WATER	\$27,220.00	\$68.05	\$41,850.75	\$14,086.35	
CONDUIT SYSTEM AND VAULTS	\$3,700.00	\$9.25	\$5,688.75	\$1,914.75	
EROSION CONTROL	\$2,930.00	\$7.33	\$4,504.88	\$1,516.28	
SIGNING AND STRIPING	\$700.00	\$1.75	\$1,076.25	\$362.25	
STREET LIGHTING, ASSUMED 1 EACH PER EVERY 150-FEET	\$4,500.00		\$18,450.00	\$6,210.00	
MISCELLANEOUS CONSTRUCTION	\$1,550.00	\$3.88	\$2,383.13	\$802.13	
LANDSCAPING	\$10,000.00	\$25.00	\$15,375.00	\$5,175.00	
<b>TOTAL CONSTRUCTION</b>			<b>\$449,165.25</b>	<b>\$151,182.45</b>	
<b>RIGHT-OF-WAY, PER S.F.</b>		<b>\$14.00</b>	<b>\$246,120.00</b>		
<b>DESIGN AND CONSTRUCTION COST</b>		<b>30.0%</b>	<b>\$134,749.58</b>	<b>\$45,354.74</b>	
<b>CONTINGENCY</b>		<b>40.0%</b>	<b>\$179,666.10</b>	<b>\$60,472.98</b>	
<b>TOTAL WITH DESIGN, CONSTRUCTION, AND CONTINGENCY</b>			<b>\$1,010,000.00</b>	<b>\$257,000.00</b>	

Appendix D

January 2009

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Subsection C3

4/22/08 EH

**SUBSECTION C4**

**PROJECT:** RESIDENTIAL STREETScape IMPROVEMENTS - PRELIMINARY COST ESTIMATE  
 RESIDENTIAL STREET STANDARDS WITH GRAVEL PARKING

**OWNER:** CITY OF WILSONVILLE

**CASE "C"**

**DATE:** APRIL 21, 2008

			ENGINEER'S EST.			
			RESIDENTIAL ROAD DESCRIPTION, BASE ON CASE "C"			
ITEM	TOTAL COST BASED ON 400- FEET ROADWAY, REFER TO PRELIMINARY BASE COST FOR CASE "C"	UNIT PRICE PER LINEAL FOOT OF ROADWAY	FOURTH STREET, FROM MAGNOLIA TO FIR	ALTERNATE ACCESS BY CHURCH, FROM BAILEY TO 5TH	ALTERNATE ACCESS TO TREATMENT PLANT, FROM 5TH TO NORTH OF TREATMENT PLANT	CONNECTION FROM 4TH TO ALTERNATE ACCESS NEAR TREATMENT PLAN
<b>ROADWAY LENGTH, FEET</b>			<b>325</b>	<b>742</b>	<b>675</b>	<b>110</b>
STREET WORK	\$149,200.00	\$373.00	\$121,225.00	\$276,766.00	\$251,775.00	\$41,030.00
CONCRETE WORK	\$30,780.00	\$76.95	\$25,008.75	\$57,096.90	\$51,941.25	\$8,464.50
STORM SEWER	\$51,460.00	\$128.65	\$41,811.25	\$95,458.30	\$86,838.75	\$14,151.50
SANITARY SEWER	\$43,100.00	\$107.75	\$35,018.75	\$79,950.50	\$72,731.25	\$11,852.50
WATER	\$25,170.00	\$62.93	\$20,450.63	\$46,690.35	\$42,474.38	\$6,921.75
CONDUIT SYSTEM AND VAULTS	\$8,900.00	\$22.25	\$7,231.25	\$16,509.50	\$15,018.75	\$2,447.50
EROSION CONTROL	\$3,730.00	\$9.33	\$3,030.63	\$6,919.15	\$6,294.38	\$1,025.75
SIGNING AND STRIPING	\$450.00	\$1.13	\$365.63	\$834.75	\$759.38	\$123.75
STREET LIGHTING, ASSUMED 1 EACH PER EVERY 150-FEET	\$4,500.00		\$9,750.00	\$22,260.00	\$20,250.00	\$3,300.00
MISCELLANEOUS CONSTRUCTION	\$3,950.00	\$9.88	\$3,209.38	\$7,327.25	\$6,665.63	\$1,086.25
LANDSCAPING	\$15,000.00	\$37.50	\$12,187.50	\$27,825.00	\$25,312.50	\$4,125.00
<b>TOTAL CONSTRUCTION</b>			<b>\$279,288.75</b>	<b>\$637,637.70</b>	<b>\$580,061.25</b>	<b>\$94,528.50</b>
<b>RIGHT-OF-WAY, PER S.F.</b>		<b>\$14.00</b>		<b>\$363,580.00</b>	<b>\$567,000.00</b>	<b>\$92,400.00</b>
<b>DESIGN AND CONSTRUCTION COST</b>		<b>30.0%</b>	<b>\$83,786.63</b>	<b>\$191,291.31</b>	<b>\$174,018.38</b>	<b>\$28,358.55</b>
<b>CONTINGENCY</b>		<b>40.0%</b>	<b>\$111,715.50</b>	<b>\$255,055.08</b>	<b>\$232,024.50</b>	<b>\$37,811.40</b>
<b>TOTAL WITH DESIGN, CONSTRUCTION, AND CONTINGENCY</b>			<b>\$475,000.00</b>	<b>\$1,448,000.00</b>	<b>\$1,553,000.00</b>	<b>\$253,000.00</b>

**SUBSECTION C5****CASE "A1":**

**RESIDENTIAL STREETScape IMPROVEMENTS - PRELIMINARY BASE COST  
ASSUMED ONE CITY BLOCK (ROUGHLY 400-FEET)  
RESIDENTIAL STREET STANDARDS WITH GRAVEL PARKING  
ROUGHLY 30-FEET OF EXISTING PAVEMENT, 18" WATER MAIN UPGRADE**

**OWNER:****CITY OF WILSONVILLE****DATE: APRIL 21, 2008**

ITEM	QUANTITY	UNIT	DESCRIPTION	ENGINEER'S EST.	
				UNIT PRICE	TOTAL

**STREET WORK**

1.		L.S.	Mobilization	\$27,700.00	\$27,700.00
2.		L.S.	Temp. Protection & Direction of Traffic	\$5,400.00	\$5,400.00
3.	640	HRS.	Flaggers	\$40.00	\$25,600.00
4.	40	Day	Portable Changeable Message Signs	\$200.00	\$8,000.00
5.		L.S.	Watering	\$500.00	\$500.00
6.		L.S.	Clearing and Grubbing	\$1,000.00	\$1,000.00
7.	465	C.Y.	Unclassified Excavation	\$14.00	\$6,510.00
8.	170	Ton	1/2" Dense Graded Level 3 A.C Pavement	\$85.00	\$14,450.00
9.	257	C.Y.	Aggregate Base	\$40.00	\$10,280.00
10.	120	C.Y.	Aggregate Shoulder Gravel for Parking Area	\$40.00	\$4,800.00
11.	20	C.Y.	Rock Excavation (If Nec.)	\$35.00	\$700.00
12.	20	C.Y.	Import Subgrade Stab. Rock (If Nec.)	\$25.00	\$500.00
13.	710	S.Y.	Subgrade Geotextile (If Nec.)	\$1.00	\$710.00
14.	800	L.F.	Asphalt Pavement Cutting	\$2.00	\$1,600.00
<b>TOTAL STREET WORK</b>					<b>\$107,800.00</b>

**CONCRETE WORK**

1.	270	S.Y.	Standard Concrete Residential Driveway	\$40.00	\$10,800.00
2.	444	S.Y.	Concrete Walks (including ramps)	\$45.00	\$19,980.00
<b>TOTAL CONCRETE WORK</b>					<b>\$30,780.00</b>

**STORM SEWER**

1.	240	L.F.	12" C-900 Storm Lateral (Imp. Backfill)	\$34.00	\$8,160.00
2.	400	L.F.	24" Storm Main	\$60.00	\$24,000.00
3.	6	EA.	Ditch Inlet	\$1,200.00	\$7,200.00
4.	1	EA.	48" Conc. Stm. MH	\$2,000.00	\$2,000.00
5.	1	EA.	Core & Connect to Ex 48" Conc. Stm. MH	\$1,500.00	\$1,500.00
6.	240	L.F.	Trench Protection	\$5.00	\$1,200.00
7.	20	C.Y.	Trench Foundation (If Nec.)	\$30.00	\$600.00
8.	15	C.Y.	Rock Excavation (If Necessary)	\$100.00	\$1,500.00
<b>TOTAL STORM SEWER</b>					<b>\$46,160.00</b>

**SANITARY**

1.	400	L.F.	8 Inch Sanitary Sewer Pipe, 10-ft Depth	\$85.00	\$34,000.00
2.	2	E.A.	Sanitary Manhole	\$2,000.00	\$4,000.00
3.	400	L.F.	Trench Protection	\$4.00	\$1,600.00
4.	25	C.Y.	Rock Excavation	\$100.00	\$2,500.00
<b>TOTAL SANITARY WORK</b>					<b>\$42,100.00</b>

## WATER

1.	400	L.F.	18 Inch D.I. Potable Water Pipes and Fittings	\$112.00	\$44,800.00	
2.		L.F.	8" Inch D.I. Potable Water Pipes and Fittings	\$52.00		
3.	2	E.A.	18" Butterfly Valves	\$2,600.00	\$5,200.00	
4.		E.A.	8" Gave Valves	\$860.00		
5.	2	E.A.	Hydrant Assemblies	\$2,050.00	\$4,100.00	
6.	400	L.F.	Trench Protection	\$1.50	\$600.00	
TOTAL WATER WORK						\$54,700.00

## CONDUIT SYSTEM AND VAULTS

1.	200	L.F.	12" PVC C-900 Conduit Sleeves	\$12.00	\$2,400.00	
2.		L.F.	Convert Overhead Utilities to Underground	\$100.00		
3.	10	C.Y.	Trench Foundation (If Nec.)	\$30.00	\$300.00	
4.	10	C.Y.	Rock Excavation (If Nec.)	\$100.00	\$1,000.00	
TOTAL CONDUIT SYSTEM AND VAULTS						\$3,700.00

## EROSION CONTROL

1.	800	L.F.	Sediment Fence, Unsupported	\$3.00	\$2,400.00	
2.	6	E.A.	Inlet Protection (Type 3)	\$55.00	\$330.00	
3.	1	L.S.	Permanent Seeding, Complete	\$200.00	\$200.00	
TOTAL EROSION CONTROL						\$2,930.00

## SIGNING AND STRIPING

1.		L.S.	Perm. Striping And Pavement Marking	\$200.00	\$200.00	
2.		L.S.	Permanent Sign Installation, Complete	\$250.00	\$250.00	
3.		L.S.	Remove Existing Signs	\$100.00	\$100.00	
4.		L.S.	Remove and Reinstall Ex. Signs	\$150.00	\$150.00	
TOTAL SIGNING AND STRIPING						\$700.00

## STREET LIGHTING

1.	1	E.A.	Roadway Lighting, Darksky Type	\$4,500.00	\$4,500.00	
TOTAL STREET LIGHTING						\$4,500.00

## MISCELLANEOUS CONSTRUCTION

1.	2	E.A.	Adjust Existing Gas Valves to F.G.	\$200.00	\$400.00	
2.		L.S.	Franchise Utility Markers to be Relocated	\$550.00	\$550.00	
3.	2	E.A.	Centerline Monument	\$300.00	\$600.00	
4.		L.F.	Orange Safety Fence	\$3.00		
TOTAL MISCELLANEOUS CONST.						\$1,550.00

## LANDSCAPING

1.		L.S.	Landscaping, Complete	\$10,000.00	\$10,000.00	
TOTAL LANDSCAPING						\$10,000.00

**SUBSECTION C6****CASE "A2":**

**RESIDENTIAL STREETScape IMPROVEMENTS - PRELIMINARY BASE COST  
ASSUMED ONE CITY BLOCK (ROUGHLY 400-FEET)  
RESIDENTIAL STREET STANDARDS WITH GRAVEL PARKING  
ROUGHLY 30-FEET OF EXISTING PAVEMENT, 8" WATER MAIN UPGRADE**

**OWNER:****CITY OF WILSONVILLE****DATE: APRIL 21, 2008**

ITEM	QUANTITY	UNIT	DESCRIPTION	ENGINEER'S EST.	
				UNIT PRICE	TOTAL

**STREET WORK**

1.		L.S.	Mobilization	\$29,100.00	\$29,100.00
2.		L.S.	Temp. Protection & Direction of Traffic	\$4,900.00	\$4,900.00
3.	640	HRS.	Flaggers	\$40.00	\$25,600.00
4.	40	Day	Portable Changeable Message Signs	\$200.00	\$8,000.00
5.		L.S.	Watering	\$500.00	\$500.00
6.		L.S.	Clearing and Grubbing	\$1,000.00	\$1,000.00
7.	465	C.Y.	Unclassified Excavation	\$14.00	\$6,510.00
8.	170	Ton	1/2" Dense Graded Level 3 A.C Pavement	\$85.00	\$14,450.00
9.	257	C.Y.	Aggregate Base	\$40.00	\$10,280.00
10.	120	C.Y.	Aggregate Shoulder Gravel for Parking Area	\$40.00	\$4,800.00
11.	20	C.Y.	Rock Excavation (If Nec.)	\$35.00	\$700.00
12.	20	C.Y.	Import Subgrade Stab. Rock (If Nec.)	\$25.00	\$500.00
13.	710	S.Y.	Subgrade Geotextile (If Nec.)	\$1.00	\$710.00
14.	800	L.F.	Asphalt Pavement Cutting	\$2.00	\$1,600.00
<b>TOTAL STREET WORK</b>					<b>\$108,700.00</b>

**CONCRETE WORK**

1.	270	S.Y.	Standard Concrete Residential Driveway	\$40.00	\$10,800.00
2.	444	S.Y.	Concrete Walks (including ramps)	\$45.00	\$19,980.00
<b>TOTAL CONCRETE WORK</b>					<b>\$30,780.00</b>

**STORM SEWER**

1.	240	L.F.	12" C-900 Storm Lateral (Imp. Backfill)	\$34.00	\$8,160.00
2.	400	L.F.	24" Storm Main	\$60.00	\$24,000.00
3.	6	EA.	Ditch Inlet	\$1,200.00	\$7,200.00
4.	1	EA.	48" Conc. Stm. MH	\$2,000.00	\$2,000.00
5.	1	EA.	Core & Connect to Ex 48" Conc. Stm. MH	\$1,500.00	\$1,500.00
6.	240	L.F.	Trench Protection	\$5.00	\$1,200.00
7.	20	C.Y.	Trench Foundation (If Nec.)	\$30.00	\$600.00
8.	15	C.Y.	Rock Excavation (If Necessary)	\$100.00	\$1,500.00
<b>TOTAL STORM SEWER</b>					<b>\$46,160.00</b>

**SANITARY**

1.	400	L.F.	8 Inch Sanitary Sewer Pipe, 10-ft Depth	\$85.00	\$34,000.00
2.	2	E.A.	Sanitary Manhole	\$2,000.00	\$4,000.00
3.	400	L.F.	Trench Protection	\$4.00	\$1,600.00
4.	25	C.Y.	Rock Excavation	\$100.00	\$2,500.00
<b>TOTAL SANITARY WORK</b>					<b>\$42,100.00</b>

## WATER

1.		L.F.	18 Inch D.I. Potable Water Pipes and Fittings	\$112.00		
2.	400	L.F.	8" Inch D.I. Potable Water Pipes and Fittings	\$52.00	\$20,800.00	
3.		E.A.	18" Butterfly Valves	\$2,600.00		
4.	2	E.A.	8" Gave Valves	\$860.00	\$1,720.00	
5.	2	E.A.	Hydrant Assemblies	\$2,050.00	\$4,100.00	
6.	400	L.F.	Trench Protection	\$1.50	\$600.00	
TOTAL WATER WORK						\$27,220.00

## CONDUIT SYSTEM AND VAULTS

1.	200	L.F.	12" PVC C-900 Conduit Sleeves	\$12.00	\$2,400.00	
2.		L.F.	Convert Overhead Utilities to Underground	\$100.00		
3.	10	C.Y.	Trench Foundation (If Nec.)	\$30.00	\$300.00	
4.	10	C.Y.	Rock Excavation (If Nec.)	\$100.00	\$1,000.00	
TOTAL CONDUIT SYSTEM AND VAULTS						\$3,700.00

## EROSION CONTROL

1.	800	L.F.	Sediment Fence, Unsupported	\$3.00	\$2,400.00	
2.	6	E.A.	Inlet Protection (Type 3)	\$55.00	\$330.00	
3.	1	L.S.	Permanent Seeding, Complete	\$200.00	\$200.00	
TOTAL EROSION CONTROL						\$2,930.00

## SIGNING AND STRIPING

1.		L.S.	Perm. Striping And Pavement Marking	\$200.00	\$200.00	
2.		L.S.	Permanent Sign Installation, Complete	\$250.00	\$250.00	
3.		L.S.	Remove Existing Signs	\$100.00	\$100.00	
4.		L.S.	Remove and Reinstall Ex. Signs	\$150.00	\$150.00	
TOTAL SIGNING AND STRIPING						\$700.00

## STREET LIGHTING

1.	1	E.A.	Roadway Lighting, Darksky Type	\$4,500.00	\$4,500.00	
TOTAL STREET LIGHTING						\$4,500.00

## MISCELLANEOUS CONSTRUCTION

1.	2	E.A.	Adjust Existing Gas Valves to F.G.	\$200.00	\$400.00	
2.		L.S.	Franchise Utility Markers to be Relocated	\$550.00	\$550.00	
3.	2	E.A.	Centerline Monument	\$300.00	\$600.00	
4.		L.F.	Orange Safety Fence	\$3.00		
TOTAL MISCELLANEOUS CONST.						\$1,550.00

## LANDSCAPING

1.		L.S.	Landscaping, Complete	\$10,000.00	\$10,000.00	
TOTAL LANDSCAPING						\$10,000.00

**SUBSECTION C7****CASE "B":**

**RESIDENTIAL STREETScape IMPROVEMENTS - PRELIMINARY BASE COST  
ASSUMED ONE CITY BLOCK (ROUGHLY 400-FEET)  
RESIDENTIAL STREET STANDARDS WITH GRAVEL PARKING  
ROUGHLY 10-20-FEET OF EXISTING PAVEMENT (ASSUMED 15-FEET)**

**OWNER:****CITY OF WILSONVILLE****DATE: APRIL 21, 2008**

ITEM	QUANTITY	UNIT	DESCRIPTION	ENGINEER'S EST.	
				UNIT PRICE	TOTAL

**STREET WORK**

1.		L.S.	Mobilization	\$25,900.00	\$25,900.00
2.		L.S.	Temp. Protection & Direction of Traffic	\$5,100.00	\$5,100.00
3.	640	HRS.	Flaggers	\$40.00	\$25,600.00
4.	40	Day	Portable Changeable Message Signs	\$200.00	\$8,000.00
5.		L.S.	Watering	\$500.00	\$500.00
6.		L.S.	Clearing and Grubbing	\$1,000.00	\$1,000.00
7.	642	C.Y.	Unclassified Excavation	\$14.00	\$8,988.00
8.	150	Ton	1/2" Dense Graded Level 3 A.C. Pavement	\$85.00	\$12,750.00
9.	29	Ton	3/4" Dense Graded Level 3 A.C. Pavement	\$85.00	\$2,465.00
10.	400	C.Y.	Aggregate Base	\$40.00	\$16,000.00
11.	120	C.Y.	Aggregate Shoulder Gravel for Parking Area	\$40.00	\$4,800.00
12.	20	C.Y.	Rock Excavation (If Nec.)	\$35.00	\$700.00
13.	20	C.Y.	Import Subgrade Stab. Rock (If Nec.)	\$25.00	\$500.00
14.	1,100	S.Y.	Subgrade Geotextile (If Nec.)	\$1.00	\$1,100.00
15.	800	L.F.	Asphalt Pavement Cutting	\$2.00	\$1,600.00
<b>TOTAL STREET WORK</b>					<b>\$115,000.00</b>

**CONCRETE WORK**

1.	270	S.Y.	Standard Concrete Residential Driveway	\$40.00	\$10,800.00
2.	444	S.Y.	Concrete Walks (including ramps)	\$45.00	\$19,980.00
<b>TOTAL CONCRETE WORK</b>					<b>\$30,780.00</b>

**STORM SEWER**

1.	240	L.F.	12" C-900 Storm Lateral (Imp. Backfill)	\$34.00	\$8,160.00
2.	400	L.F.	24" Storm Main	\$60.00	\$24,000.00
3.	6	EA.	Ditch Inlet	\$1,200.00	\$7,200.00
4.	1	EA.	48" Conc. Stm. MH	\$2,000.00	\$2,000.00
5.	1	EA.	Core & Connect to Ex 48" Conc. Stm. MH	\$1,500.00	\$1,500.00
6.	240	L.F.	Trench Protection	\$5.00	\$1,200.00
7.	20	C.Y.	Trench Foundation (If Nec.)	\$30.00	\$600.00
8.	15	C.Y.	Rock Excavation (If Necessary)	\$100.00	\$1,500.00
<b>TOTAL STORM SEWER</b>					<b>\$46,160.00</b>

**SANITARY**

1.	400	L.F.	8 Inch Sanitary Sewer Pipe, 10-ft Depth	\$85.00	\$34,000.00
2.	2	E.A.	Sanitary Manhole	\$2,000.00	\$4,000.00
3.	400	L.F.	Trench Protection	\$4.00	\$1,600.00
4.	25	C.Y.	Rock Excavation	\$100.00	\$2,500.00
<b>TOTAL SANITARY WORK</b>					<b>\$42,100.00</b>



## WATER

1.		L.F.	18 Inch D.I. Potable Water Pipes and Fittings	\$112.00		
2.	400	L.F.	8" Inch D.I. Potable Water Pipes and Fittings	\$52.00	\$20,800.00	
3.		E.A.	18" Butterfly Valves	\$2,600.00		
4.	2	E.A.	8" Gave Valves	\$860.00	\$1,720.00	
5.	2	E.A.	Hydrant Assemblies	\$2,050.00	\$4,100.00	
6.	400	L.F.	Trench Protection	\$1.50	\$600.00	
TOTAL WATER WORK						\$27,220.00

## CONDUIT SYSTEM AND VAULTS

1.	200	L.F.	12" PVC C-900 Conduit Sleeves	\$12.00	\$2,400.00	
2.		L.F.	Convert Overhead Utilities to Underground	\$100.00		
3.	10	C.Y.	Trench Foundation (If Nec.)	\$30.00	\$300.00	
4.	10	C.Y.	Rock Excavation (If Nec.)	\$100.00	\$1,000.00	
TOTAL CONDUIT SYSTEM AND VAULTS						\$3,700.00

## EROSION CONTROL

1.	800	L.F.	Sediment Fence, Unsupported	\$3.00	\$2,400.00	
2.	6	E.A.	Inlet Protection (Type 3)	\$55.00	\$330.00	
3.	1	L.S.	Permanent Seeding, Complete	\$200.00	\$200.00	
TOTAL EROSION CONTROL						\$2,930.00

## SIGNING AND STRIPING

1.		L.S.	Perm. Striping And Pavement Marking	\$200.00	\$200.00	
2.		L.S.	Permanent Sign Installation, Complete	\$250.00	\$250.00	
3.		L.S.	Remove Existing Signs	\$100.00	\$100.00	
4.		L.S.	Remove and Reinstall Ex. Signs	\$150.00	\$150.00	
TOTAL SIGNING AND STRIPING						\$700.00

## STREET LIGHTING

1.	1	E.A.	Roadway Lighting, Darksky Type	\$4,500.00	\$4,500.00	
TOTAL STREET LIGHTING						\$4,500.00

## MISCELLANEOUS CONSTRUCTION

1.	2	E.A.	Adjust Existing Gas Valves to F.G.	\$200.00	\$400.00	
2.		L.S.	Franchise Utility Markers to be Relocated	\$550.00	\$550.00	
3.	2	E.A.	Centerline Monument	\$300.00	\$600.00	
4.		L.F.	Orange Safety Fence	\$3.00		
TOTAL MISCELLANEOUS CONST.						\$1,550.00

## LANDSCAPING

1.		L.S.	Landscaping, Complete	\$10,000.00	\$10,000.00	
TOTAL LANDSCAPING						\$10,000.00

**SUBSECTION C8****CASE "C":**

**RESIDENTIAL STREETScape IMPROVEMENTS - PRELIMINARY BASE COST  
 ASSUMED ONE CITY BLOCK (ROUGHLY 400-FeET)  
 RESIDENTIAL STREET STANDARDS WITH GRAVEL PARKING  
 ASSUMED NEW ROAD IMPROVEMENTS**

**OWNER:****CITY OF WILSONVILLE****DATE: APRIL 21, 2008**

ITEM	QUANTITY	UNIT	DESCRIPTION	ENGINEER'S EST.	
				UNIT PRICE	TOTAL

**STREET WORK**

1.		L.S.	Mobilization	\$30,600.00	\$30,600.00
2.		L.S.	Temp. Protection & Direction of Traffic	\$6,000.00	\$6,000.00
3.	640	HRS.	Flaggers	\$40.00	\$25,600.00
4.	40	Day	Portable Changeable Message Signs	\$200.00	\$8,000.00
5.		L.S.	Watering	\$500.00	\$500.00
6.		L.S.	Clearing and Grubbing	\$1,000.00	\$1,000.00
7.	940	C.Y.	Unclassified Excavation	\$14.00	\$13,160.00
8.	114	Ton	1/2" Dense Graded Level 3 A.C. Pavement	\$85.00	\$9,690.00
9.	227	Ton	3/4" Dense Graded Level 3 A.C. Pavement	\$85.00	\$19,295.00
10.	642	C.Y.	Aggregate Base	\$40.00	\$25,680.00
11.	120	C.Y.	Aggregate Shoulder Gravel for Parking Area	\$40.00	\$4,800.00
12.	50	C.Y.	Rock Excavation (If Nec.)	\$35.00	\$1,750.00
13.	50	C.Y.	Import Subgrade Stab. Rock (If Nec.)	\$25.00	\$1,250.00
14.	1,780	S.Y.	Subgrade Geotextile (If Nec.)	\$1.00	\$1,780.00
15.	50	L.F.	Asphalt Pavement Cutting	\$2.00	\$100.00
<b>TOTAL STREET WORK</b>					<b>\$149,200.00</b>

**CONCRETE WORK**

1.	270	S.Y.	Standard Concrete Residential Driveway	\$40.00	\$10,800.00
2.	444	S.Y.	Concrete Walks (including ramps)	\$45.00	\$19,980.00
<b>TOTAL CONCRETE WORK</b>					<b>\$30,780.00</b>

**STORM SEWER**

1.	240	L.F.	12" C-900 Storm Lateral (Imp. Backfill)	\$34.00	\$8,160.00
2.	400	L.F.	24" Storm Main	\$60.00	\$24,000.00
3.	6	EA.	Ditch Inlet	\$1,200.00	\$7,200.00
4.	3	EA.	48" Conc. Stm. MH	\$2,000.00	\$6,000.00
5.	1	EA.	Core & Connect to Ex 48" Conc. Stm. MH	\$1,500.00	\$1,500.00
6.	400	L.F.	Trench Protection	\$5.00	\$2,000.00
7.	20	C.Y.	Trench Foundation (If Nec.)	\$30.00	\$600.00
8.	20	C.Y.	Rock Excavation (If Necessary)	\$100.00	\$2,000.00
<b>TOTAL STORM SEWER</b>					<b>\$51,460.00</b>

## SANITARY SEWER

1.	400	L.F.	8" Sanitary Sewer Pipe (10' deep)	\$85.00	\$34,000.00
2.	3	EA.	Sanitary MH	\$2,000.00	\$6,000.00
3.	400	L.F.	Trench Protection	\$4.00	\$1,600.00
4.	15	C.Y.	Rock Excavation (If Necessary)	\$100.00	\$1,500.00
TOTAL SANITARY SEWER					\$43,100.00

## WATER SUPPLY SYSTEMS

1.	400	LF	8" Water Main	\$52.00	\$20,800.00
2.	2	EA	8" Gate Valve	\$860.00	\$1,720.00
3.	1	EA	Hydrant Assembly	\$2,050.00	\$2,050.00
4.	400	LF	Trench Protection	\$1.50	\$600.00
TOTAL WATER SUPPLY SYSTEM					\$25,170.00

## CONDUIT SYSTEM AND VAULTS

1.	200	L.F.	12" PVC C-900 Conduit Sleeves	\$12.00	\$2,400.00
2.		L.F.	Convert Overhead Utilities to Underground	\$100.00	
3.	50	C.Y.	Trench Foundation (If Nec.)	\$30.00	\$1,500.00
4.	50	C.Y.	Rock Excavation (If Nec.)	\$100.00	\$5,000.00
TOTAL CONDUIT SYSTEM AND VAULTS					\$8,900.00

## EROSION CONTROL

1.	800	L.F.	Sediment Fence, Unsupported	\$3.00	\$2,400.00
2.	6	EA.	Inlet Protection (Type 3)	\$55.00	\$330.00
3.	1	L.S.	Permanent Seeding, Complete	\$1,000.00	\$1,000.00
TOTAL EROSION CONTROL					\$3,730.00

## SIGNING AND STRIPING

1.		L.S.	Perm. Striping And Pavement Marking	\$200.00	\$200.00
2.		L.S.	Permanent Sign Installation, Complete	\$250.00	\$250.00
3.		L.S.	Remove Existing Signs		
4.		L.S.	Remove and Reinstall Ex. Signs		
TOTAL SIGNING AND STRIPING					\$450.00

## STREET LIGHTING

1.	1	E.A	Roadway Lighting, Darksky Type	\$4,500.00	\$4,500.00
TOTAL STREET LIGHTING					\$4,500.00

## MISCELLANEOUS CONSTRUCTION

1.	2	EA.	Adjust Existing Gas Valves to F.G.	\$200.00	\$400.00
2.		L.S.	Franchise Utility Markers to be Relocated	\$550.00	\$550.00
3.	2	EA.	Centerline Monument	\$300.00	\$600.00
4.	800	L.F.	Orange Safety Fence	\$3.00	\$2,400.00
TOTAL MISCELLANEOUS CONST.					\$3,950.00

## LANDSCAPING

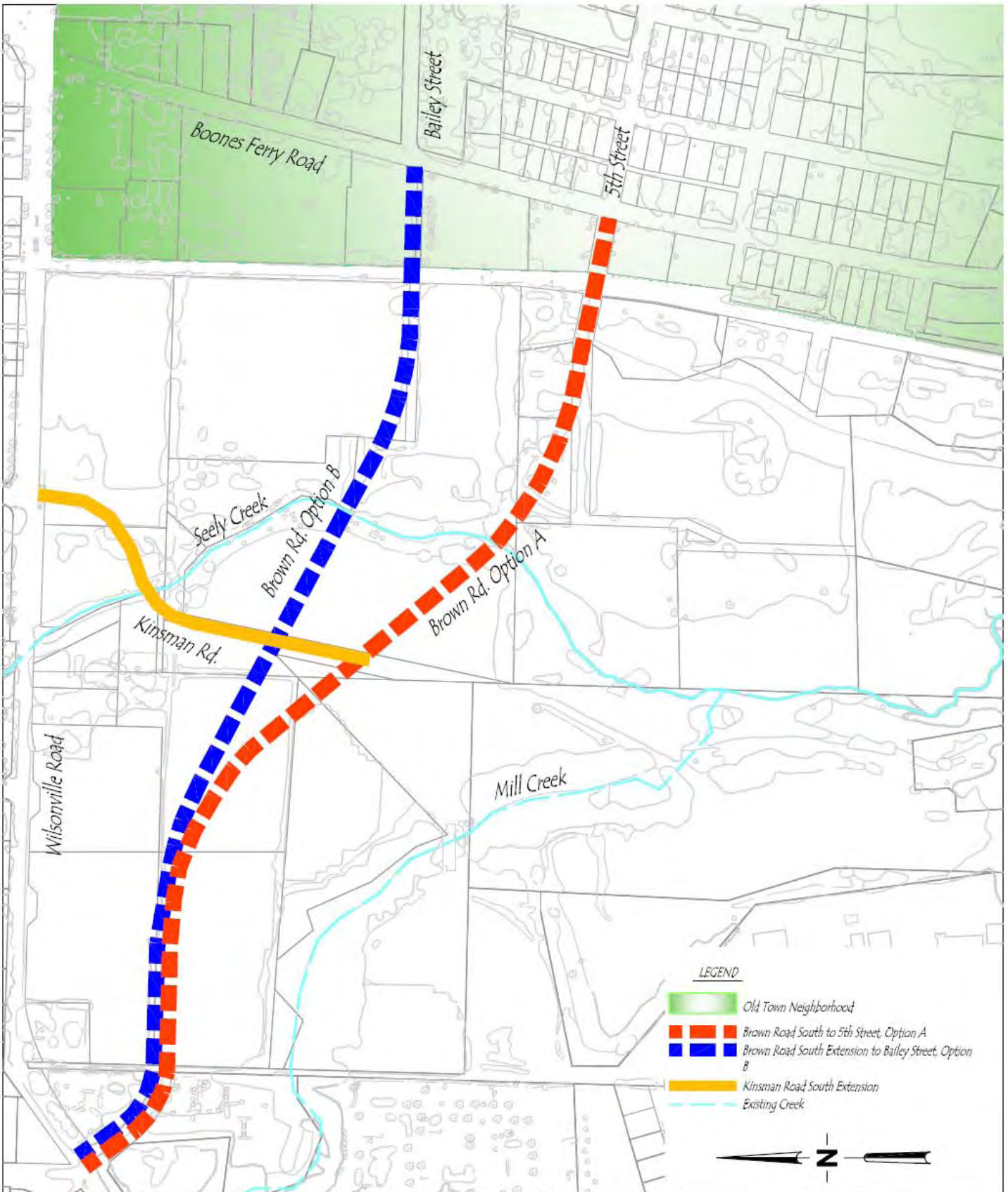
1.		L.S.	Landscaping, Complete	\$15,000.00	\$15,000.00
TOTAL LANDSCAPING					\$15,000.00

# **APPENDIX D**

## **Proposed Capital Projects Preliminary Cost Estimates June 2008**

### **Subsection D: Brown Road South Extension**





**OLD TOWN PRELIMINARY COST ESTIMATES**  
**OLD TOWN**  
**BROWN ROAD SOUTH EXTENSION**

M&S PROJECT #: 14892
DRAWN BY: <u>EH/MW</u>
CHECKED BY: <u>RP</u>
DATE: <u>06/16/08</u>
SHEET NO. <u>D1</u>

Scale: 1" = 500'

**SUBSECTION D2**

**PROJECT: BROWN ROAD SOUTH EXTENSION, OPTION A (SOUTH)  
FROM WILSONVILLE ROAD TO FIFTH STREET  
MINOR COLLECTOR WITH NO PARKING**

**OWNER: CITY OF WILSONVILLE**

**DATE: MAY 6, 2008**

**ENGINEER'S ESTIMATES**

			FROM WILSONVILLE ROAD TO FIFTH STREET TOTAL LENGTH, 3,550 FT				WILSONVILLE RD TO KINSMAN LENGTH, 1,750 FT	KINSMAN TO 5TH ST. LENGTH, 1,800 FT
ITEM	SECTION NUMBER	DESCRIPTION	UNIT	QUANTITY	UNIT COST	TOTAL	49% OF TOTAL OR AS NOTED	51% OF TOTAL OR AS NOTED
<b>PART 00150 - CONTROL OF WORK</b>								
1.	00150	EXTENDED MAINTENANCE (2YR)	LS		\$2,000.00	\$2,000	\$2,000.00	\$2,000.00
<b>SUBTOTAL</b>						\$2,000	\$2,000	\$2,000
<b>PART 00200 - TEMPORARY FEATURES AND APPURTENANCES</b>								
2.	00210	MOBILIZATION	LS			\$472,405	\$215,151	\$302,893
3.	00225	TEMPORARY PROTECTION AND DIRECTION OF TRAFFIC	LS	1	\$8,000.00	\$8,000	\$3,920.00	\$4,080.00
4.	00225	TEMPORARY SIGNS	SF	320	\$17.00	\$5,440	\$2,665.60	\$2,774.40
5.	00225	TEMPORARY BARRICADES, TYPE III	FT	100	\$65.00	\$6,500.00	\$6,500.00	\$6,500.00
6.	00225	PORTABLE CHANGEABLE MESSAGE BORD SIGNS	DAY	120	\$200.00	\$24,000.00	\$24,000.00	\$24,000.00
7.	00225	FLAGGERS (TRAFFIC CONTROL LABOR)	HR	1600	\$40.00	\$64,000	\$64,000	\$64,000
<b>SUBTOTAL</b>						\$580,345	\$316,237	\$404,248
<b>EROSION CONTROL</b>								
8.	00280	TEMPORARY TYPE ORANGE PLASTIC MESH FENCE	LF	7000	\$2.50	\$17,500	\$8,575.00	\$8,925.00
9.	00280	CONSTRUCTION ENTRANCES	EA	2	\$1,145.00	\$2,290	\$2,290	\$2,290
10.	00280	INLET PROTECTION	EA	23	\$55.00	\$1,265	\$605.00	\$660.00
11.	00280	TIRE WASH FACILITY	EA	2	\$2,000.00	\$4,000	\$4,000	\$4,000
12.	00280	BIOFILTER BAGS	EA	92	\$14.50	\$1,334	\$652.50	\$681.50
13.	00280	SEDIMENT FENCE, UNSUPPORTED	LF	7000	\$3.00	\$21,000	\$10,290.00	\$10,710.00
14.	00280	TEMPORARY SEEDING	AC	2	\$2,530.00	\$5,060	\$2,479.40	\$2,580.60
15.	00280	SAWCUT EXISTING PAVEMENT	LF	200	\$2.00	\$400	\$196.00	\$204.00
16.	00280	REMOVAL OF SURFACINGS	SY	3400	\$4.00	\$13,600	\$6,664.00	\$6,936.00
<b>SUBTOTAL</b>						\$66,449	\$35,751.90	\$36,987.10

**PART 00300 - ROADWORK**

17.	00310	REMOVAL OF STRUCTURES AND OBSTRUCTIONS	LS	1	\$25,000.00	\$25,000	\$12,250.00	\$12,750.00
18.	00320	CLEARING AND GRUBBING	LS	1	\$50,000.00	\$50,000	\$24,500.00	\$25,500.00
19.	00330	COMMON EXCAVATION	CY	20000	\$14.00	\$280,000	\$137,200.00	\$142,800.00
20.	00331	SUBGRADE STABILIZATION (IF NECESSARY)	CY	3290	\$25.00	\$82,250	\$40,302.50	\$41,947.50
21.	00340	WATERING	LS	1	\$10,000.00	\$10,000	\$4,900.00	\$5,100.00
22.	00350	GEOSYNTHETIC INSTALLATION	SY	20000	\$1.00	\$20,000	\$9,800.00	\$10,200.00
<b>SUBTOTAL</b>						<b>\$467,250</b>	<b>\$228,952.50</b>	<b>\$238,297.50</b>

**PART 00400 - DRAINAGE AND SEWERS****STORM SEWERS**

23.	00470	STORM MANHOLE	EA	7	\$2,000.00	\$14,000	\$6,000.00	\$8,000.00
24.	00470	CONCRETE INLETS, TYPE CG-30	EA	18	\$1,300.00	\$23,400	\$11,700.00	\$11,700.00
25.	00470	10" STORM PIPE	LF	400	\$40.00	\$16,000	\$7,840.00	\$8,160.00
26.	00470	24" STORM PIPE	LF	2800	\$60.00	\$168,000	\$82,320.00	\$85,680.00
27.	00470	6'x4' BOX CULVERT WITH 8' HEAD WALLS	EA	2	\$150,000.00	\$300,000	\$0.00	\$300,000
28.		TRENCH PROTECTION	LF	2900	\$2.00	\$5,800	\$2,842.00	\$2,958.00
<b>SUBTOTAL</b>						<b>\$527,200</b>	<b>\$110,702</b>	<b>\$416,498</b>

**SANITARY SEWERS**

29.	00445	8 INCH SANITARY SEWER PIPE, 10 FT DEPTH	LF	2000	\$85.00	\$170,000	\$83,300.00	\$86,700.00
30.		SANITARY MANHOLE	EA	6	\$2,000.00	\$12,000	\$6,000.00	\$6,000.00
31.		TRENCH PROTECTION	LF	2000	\$4.00	\$8,000	\$3,920.00	\$4,080.00
<b>SUBTOTAL</b>						<b>\$190,000</b>	<b>\$93,220</b>	<b>\$96,780</b>

**UTILITY CONDUITS & VAULTS**

32.	00445	2 INCH PVC PIPE, 5 FT DEPTH	LF	13000	\$2.00	\$26,000	\$12,740.00	\$13,260.00
33.	00445	4 INCH PVC PIPE, 5 FT DEPTH	LF	24000	\$2.50	\$60,000	\$29,400.00	\$30,600.00
34.		UTILITY TRENCH EXCAVATION	LF	4500	\$30.00	\$135,000	\$66,150.00	\$68,850.00
35.	00470	RAILROAD TRACK CROSSING	LS	1	\$50,000.00	\$50,000	\$0.00	\$50,000
36.	00470	UTILITY VAULT, 12' X 6' X 8'	EA	3	\$4,600.00	\$13,800	\$4,600.00	\$9,200.00
37.	00470	UTILITY VAULT, 2' X 3'	EA	9	\$800.00	\$7,200	\$3,200.00	\$4,000.00
<b>SUBTOTAL</b>						<b>\$292,000</b>	<b>\$116,090</b>	<b>\$175,910</b>

**PART 00600 - BASES**

38.	00640	AGGREGATE BASE	CY	4500	\$40.00	\$180,000	\$88,200.00	\$91,800.00
<b>SUBTOTAL</b>						<b>\$180,000</b>	<b>\$88,200</b>	<b>\$91,800</b>



**PART 00700 - WEARING SURFACES**

39.	00744	ASPHALT APPROACHES	CY	100	\$650.00	\$65,000	\$31,850.00	\$33,150.00
40.	00759	CURB AND GUTTER CONCRETE CURBS	LF	7100	\$14.00	\$99,400	\$48,706.00	\$50,694.00
41.	00759	CONCRETE DRIVEWAYS	SY	900	\$75.00	\$67,500	\$33,075.00	\$34,425.00
42.	00759	CONCRETE WALKS	SY	3945	\$45.00	\$177,525	\$86,987.25	\$90,537.75
43.	00755	7" PCC PAVEMENT	SF	165800	\$6.00	\$994,800	\$487,452.00	\$507,348.00
<b>SUBTOTAL</b>						<b>\$1,404,225</b>	<b>\$688,070</b>	<b>\$716,155</b>

**PART 00800 - PERMANENT TRAFFIC SAFETY AND GUIDANCE DEVICES**

44.	00850	PAVEMENT LEGEND, TYPE A: ARROWS	EA	20	\$100.00	\$2,000	\$1,000.00	\$1,000.00
45.	00850	PAVEMENT LEGEND, TYPE A: BICYCLE LANE SYMBOLS	EA	20	\$125.00	\$2,500	\$1,250.00	\$1,250.00
46.	00861	PAINTED PERMANENT PAVEMENT STRIPING	LF	22000	\$0.10	\$2,200	\$1,078.00	\$1,122.00
47.	00863	STRIPING AND STRIPE REMOVAL MOBILIZATION	EA	1	\$200.00	\$200	\$200.00	\$200.00
48.	00850	PVMNT LEGEND; TYPE B "ONLY"	EA	2	\$275.00	\$550	\$275.00	\$275.00
49.	00860	BI DIRECTIONAL TYPE I YELLOW MARKERS	EA	360	\$4.00	\$1,440	\$704.00	\$736.00
50.	00860	WHITE TYPE II MARKERS	EA	200	\$3.00	\$600	\$294.00	\$306.00
<b>SUBTOTAL</b>						<b>\$9,490</b>	<b>\$4,801</b>	<b>\$4,889</b>

**PART 00900 - PERMANENT TRAFFIC CONTROL AND ILLUMINATION SYSTEMS**

51.	00930	SIGN AND LUMINAIRE SUPPORTS	LS	1	\$2,000.00	\$2,000	\$2,000	\$2,000
52.	00990	TRAFFIC SIGNAL INSTALLATION	EA	1	\$350,000.00	\$350,000	\$350,000	\$350,000
53.	00960	COBRA HEAD LUMINAIRES, POLES, FOUNDATIONS, JUNCTION	EA	50	\$3,400.00	\$170,000	\$85,000.00	\$88,400.00
54.	00990	ILLUMINATION SERVICE CABINET AND EQUIPMENT	EA	2	\$7,500.00	\$15,000	\$7,500.00	\$7,500.00
<b>SUBTOTAL</b>						<b>\$537,000</b>	<b>\$444,500</b>	<b>\$447,900</b>

**PART 01000 - RIGHT OF WAY DEVELOPMENT AND CONTROL**

55.	01040	LANDSCAPING	LS	1	\$210,000.00	\$210,000	\$102,900.00	\$107,100.00
56.	01040	WETLAND MITIGATION	AC	0.60	\$5,000.00	\$3,000	\$0.00	\$3,000
57.	01040	RAILROAD SURFACE CROSSING	LS	1	\$400,000.00	\$400,000	\$0.00	\$400,000
<b>SUBTOTAL</b>						<b>\$613,000</b>	<b>\$102,900</b>	<b>\$510,100</b>

**PART 01100 - WATER SUPPLY SYSTEMS**

58.	01120	IRRIGATION	LS	1	\$60,000.00	\$60,000	\$29,400.00	\$30,600.00
59.	01170	2 INCH POTABLE WATER PIPE, FITTINGS AND COUPLINGS	LF	700	\$30.00	\$21,000	\$10,290.00	\$10,710.00
60.	01170	12 INCH POTABLE WATER PIPE, FITTINGS AND COUPLINGS	LF	3350	\$49.00	\$164,150	\$80,433.50	\$83,716.50
61.	01150	6 INCH GATE VALVE	EA	8	\$475.00	\$3,800	\$1,900.00	\$1,900.00
62.	01150	12 INCH GATE VALVE	EA	8	\$1,000.00	\$8,000	\$4,000.00	\$4,000.00
63.	01160	HYDRANT ASSEMBLIES	EA	8	\$2,050.00	\$16,400	\$8,200.00	\$8,200.00
64.		RAILROAD TRACK CROSSING	LS	1	\$50,000.00	\$50,000	\$0.00	\$50,000
65.		TRENCH PROTECTION	LF	4100	\$1.50	\$6,150	\$3,013.50	\$3,136.50
<b>SUBTOTAL</b>						<b>\$329,500</b>	<b>\$137,237</b>	<b>\$192,263</b>

**SUBTOTAL OF CONSTRUCTION ITEMS**

<b>\$5,198,459</b>	<b>\$2,368,661</b>	<b>\$3,333,827</b>
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**DESIGN, ROW & CONTINGENCIES**

		RIGHT OF WAY (Residential)	SF	284,000	\$14.00	\$3,976,000	\$1,948,240.00	\$2,027,760.00
		TEMPORARY CONSTRUCTION EASEMENT	SF	35,500	\$1.00	\$35,500	\$17,395.00	\$18,105.00
		DESIGN AND CONST. MGMT. (25%)				\$1,299,615	\$592,165	\$833,457
		CONTINGENCIES (30%)				\$1,559,538	\$710,598	\$1,000,148
<b>SUBTOTAL INCLUDING ROW, DESIGN, AND CONTINGENCIES</b>						<b>\$6,870,653</b>	<b>\$3,268,399</b>	<b>\$3,879,470</b>

**TOTAL PRELIMINARY COST ESTIMATE**

<b>\$12,069,000</b>	<b>\$5,637,000</b>	<b>\$7,213,000</b>
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**SUBSECTION D3**

**PROJECT: BROWN ROAD SOUTH EXTENSION, OPTION B (NORTH)  
FROM WILSONVILLE ROAD TO BAILEY STREET  
MINOR COLLECTOR WITH NO PARKING**

**OWNER: CITY OF WILSONVILLE**

**DATE: APRIL 18, 2008**

**ENGINEER'S ESTIMATES**

ITEM	SECTION NUMBER	DESCRIPTION	UNIT	QUANTITY	UNIT COST	TOTAL	FROM WILSONVILLE ROAD TO BAILEY STREET TOTAL LENGTH, 3,400 FT		WILSONVILLE RD TO KINSMAN LENGTH, 1,810 FT	KINSMAN TO BAILEY ST. LENGTH, 1,590 FT
							53% OF TOTAL OR AS NOTED	47% OF TOTAL OR AS NOTED		

**PART 00150 - CONTROL OF WORK**

1.	00150	EXTENDED MAINTENANCE (2YR)	LS		\$2,000.00	\$2,000	\$2,000.00	\$2,000.00	\$2,000.00
<b>SUBTOTAL</b>						\$2,000	\$2,000	\$2,000	\$2,000

**PART 00200 - TEMPORARY FEATURES AND APPURTENANCES**

2.	00210	MOBILIZATION	LS			\$450,556	\$225,225	\$270,630
3.	00225	TEMPORARY PROTECTION AND DIRECTION OF TRAFFIC	LS	1	\$8,000.00	\$8,000	\$4,240.00	\$3,760.00
4.	00225	TEMPORARY SIGNS	SF	320	\$17.00	\$5,440	\$2,883.20	\$2,556.80
5.	00225	TEMPORARY BARRICADES, TYPE III	FT	100	\$65.00	\$6,500.00	\$6,500.00	\$6,500.00
6.	00225	PORTABLE CHANGEABLE MESSAGE BORD SIGNS	DAY	120	\$200.00	\$24,000.00	\$24,000.00	\$24,000.00
7.	00225	FLAGGERS (TRAFFIC CONTROL LABOR)	HR	1600	\$40.00	\$64,000	\$64,000	\$64,000
<b>SUBTOTAL</b>						\$558,496	\$326,848	\$371,447

**EROSION CONTROL**

8.	00280	TEMPORARY TYPE ORANGE PLASTIC MESH FENCE	LF	7000	\$2.50	\$17,500	\$9,275.00	\$8,225.00
9.	00280	CONSTRUCTION ENTRANCES	EA	2	\$1,145.00	\$2,290	\$2,290	\$2,290
10.	00280	INLET PROTECTION	EA	19	\$55.00	\$1,045	\$550.00	\$495.00
11.	00280	TIRE WASH FACILITY	EA	2	\$2,000.00	\$4,000	\$4,000	\$4,000
12.	00280	BIOFILTER BAGS	EA	76	\$14.50	\$1,102	\$580.00	\$522.00
13.	00280	SEDIMENT FENCE, UNSUPPORTED	LF	7000	\$3.00	\$21,000	\$11,130.00	\$9,870.00
14.	00280	TEMPORARY SEEDING	AC	2	\$2,530.00	\$5,060	\$2,681.80	\$2,378.20
15.	00280	SAWCUT EXISTING PAVEMENT	LF	200	\$2.00	\$400	\$212.00	\$188.00
16.	00280	REMOVAL OF SURFACINGS	SY	2000	\$4.00	\$8,000	\$4,240.00	\$3,760.00
<b>SUBTOTAL</b>						\$60,397	\$34,958.80	\$31,728.20

**PART 00300 - ROADWORK**

17.	00310	REMOVAL OF STRUCTURES AND OBSTRUCTIONS	LS	1	\$25,000.00	\$25,000	\$13,250.00	\$11,750.00
18.	00320	CLEARING AND GRUBBING	LS	1	\$50,000.00	\$50,000	\$26,500.00	\$23,500.00
19.	00330	COMMON EXCAVATION	CY	19500	\$14.00	\$273,000	\$144,690.00	\$128,310.00
20.	00331	SUBGRADE STABILIZATION	CY	3150	\$25.00	\$78,750	\$41,737.50	\$37,012.50
21.	00340	WATERING	LS	1	\$10,000.00	\$10,000	\$5,300.00	\$4,700.00
22.	00350	GEOSYNTHETIC INSTALLATION	SY	19000	\$1.00	\$19,000	\$10,070.00	\$8,930.00
<b>SUBTOTAL</b>						<b>\$455,750</b>	<b>\$241,547.50</b>	<b>\$214,202.50</b>

**PART 00400 - DRAINAGE AND SEWERS****STORM SEWERS**

23.	00470	STORM MANHOLE	EA	6	\$2,000.00	\$12,000	\$6,000.00	\$6,000.00
24.	00470	CONCRETE INLETS, TYPE CG-30	EA	14	\$1,300.00	\$18,200	\$9,100.00	\$9,100.00
25.	00470	10" STORM PIPE	LF	350	\$40.00	\$14,000	\$7,420.00	\$6,580.00
26.	00470	24" STORM PIPE	LF	2250	\$60.00	\$135,000	\$71,550.00	\$63,450.00
27.	00470	6'x4' BOX CULVERT WITH 8' HEAD WALLS	EA	1	\$150,000.00	\$150,000	\$0.00	\$150,000
28.		TRENCH PROTECTION	LF	2700	\$2.00	\$5,400	\$2,862.00	\$2,538.00
<b>SUBTOTAL</b>						<b>\$334,600</b>	<b>\$96,932</b>	<b>\$237,668</b>

**SANITARY SEWERS**

29.	00445	8 INCH SANITARY SEWER PIPE, 10 FT DEPTH	LF	2000	\$85.00	\$170,000	\$90,100.00	\$79,900.00
30.		SANITARY MANHOLE	EA	6	\$2,000.00	\$12,000	\$6,000.00	\$6,000.00
31.		TRENCH PROTECTION	LF	2000	\$4.00	\$8,000	\$4,240.00	\$3,760.00
<b>SUBTOTAL</b>						<b>\$190,000</b>	<b>\$100,340</b>	<b>\$89,660</b>

**UTILITY CONDUITS & VAULTS**

32.	00445	2 INCH PVC PIPE, 5 FT DEPTH	LF	13000	\$2.00	\$26,000	\$13,780.00	\$12,220.00
33.	00445	4 INCH PVC PIPE, 5 FT DEPTH	LF	24000	\$2.50	\$60,000	\$31,800.00	\$28,200.00
34.		UTILITY TRENCH EXCAVATION	LF	4500	\$30.00	\$135,000	\$71,550.00	\$63,450.00
35.	00470	RAILROAD TRACK CROSSING	LS	1	\$50,000.00	\$50,000	\$0.00	\$50,000
36.	00470	UTILITY VAULT, 12' X 6' X 8'	EA	3	\$4,600.00	\$13,800	\$9,200.00	\$4,600.00
37.	00470	UTILITY VAULT, 2' X 3'	EA	9	\$800.00	\$7,200	\$4,000.00	\$3,200.00
<b>SUBTOTAL</b>						<b>\$292,000</b>	<b>\$130,330</b>	<b>\$161,670</b>

**PART 00600 - BASES**

38.	00640	AGGREGATE BASE	CY	6250	\$40.00	\$250,000	\$132,500.00	\$117,500.00
<b>SUBTOTAL</b>						<b>\$250,000</b>	<b>\$132,500</b>	<b>\$117,500</b>

**PART 00700 - WEARING SURFACES**

39.	00744	ASPHALT APPROACHES	CY	100	\$650.00	\$65,000	\$34,450.00	\$30,550.00
40.	00759	CURB AND GUTTER CONCRETE CURBS	LF	6800	\$14.00	\$95,200	\$50,456.00	\$44,744.00
41.	00759	CONCRETE DRIVEWAYS	SY	900	\$75.00	\$67,500	\$35,775.00	\$31,725.00
42.	00759	CONCRETE WALKS	SY	3780	\$45.00	\$170,100	\$90,153.00	\$79,947.00
43.	00755	7" PCC PAVEMENT	SF	158000	\$6.00	\$948,000	\$502,440.00	\$445,560.00
<b>SUBTOTAL</b>						<b>\$1,345,800</b>	<b>\$713,274</b>	<b>\$632,526</b>

**PART 00800 - PERMANENT TRAFFIC SAFETY AND GUIDANCE DEVICES**

44.	00850	PAVEMENT LEGEND, TYPE A: ARROWS	EA	20	\$100.00	\$2,000	\$1,100.00	\$900.00
45.	00850	PAVEMENT LEGEND, TYPE A: BICYCLE LANE SYMBOLS	EA	20	\$125.00	\$2,500	\$1,375.00	\$1,125.00
46.	00861	PAINTED PERMANENT PAVEMENT STRIPING	LF	22000	\$0.10	\$2,200	\$1,166.00	\$1,034.00
47.	00863	STRIPING AND STRIPE REMOVAL MOBILIZATION	EA	1	\$200.00	\$200	\$200.00	\$200.00
48.	00850	PVMNT LEGEND; TYPE B "ONLY"	EA	2	\$275.00	\$550	\$275.00	\$275.00
49.	00860	BI DIRECTIONAL TYPE I YELLOW MARKERS	EA	340	\$4.00	\$1,360	\$720.00	\$640.00
50.	00860	WHITE TYPE II MARKERS	EA	170	\$3.00	\$510	\$270.00	\$240.00
<b>SUBTOTAL</b>						<b>\$9,320</b>	<b>\$5,106</b>	<b>\$4,414</b>

**PART 00900 - PERMANENT TRAFFIC CONTROL AND ILLUMINATION SYSTEMS**

51.	00930	SIGN AND LUMINAIRE SUPPORTS	LS	1	\$2,000.00	\$2,000	\$2,000	\$2,000
52.	00990	TRAFFIC SIGNAL INSTALLATION	EA	1	\$350,000.00	\$350,000	\$350,000	\$350,000
53.	00960	COBRA HEAD LUMINAIRES, POLES, FOUNDATIONS, JUNCTION	EA	46	\$3,400.00	\$156,400	\$81,600.00	\$74,800.00
54.	00990	ILLUMINATION SERVICE CABINET AND EQUIPMENT	EA	2	\$7,500.00	\$15,000	\$7,500.00	\$7,500.00
<b>SUBTOTAL</b>						<b>\$523,400</b>	<b>\$441,100</b>	<b>\$434,300</b>

**PART 01000 - RIGHT OF WAY DEVELOPMENT AND CONTROL**

55.	01040	LANDSCAPING	LS	1	\$210,000.00	\$210,000	\$111,300.00	\$98,700.00
56.	01040	WETLAND MITIGATION	AC	0.90	\$5,000.00	\$4,500	\$0.00	\$4,500
57.	01040	RAILROAD SURFACE CROSSING	LS	1	\$400,000.00	\$400,000	\$0.00	\$400,000
<b>SUBTOTAL</b>						<b>\$614,500</b>	<b>\$111,300</b>	<b>\$503,200</b>

**PART 01100 - WATER SUPPLY SYSTEMS**

58.	01120	IRRIGATION	LS	1	\$60,000.00	\$60,000	\$31,800.00	\$28,200.00
59.	01170	2 INCH POTABLE WATER PIPE, FITTINGS AND COUPLINGS	LF	700	\$30.00	\$21,000	\$11,130.00	\$9,870.00
60.	01170	12 INCH POTABLE WATER PIPE, FITTINGS AND COUPLINGS	LF	3200	\$49.00	\$156,800	\$83,104.00	\$73,696.00
61.	01150	6 INCH GATE VALVE	EA	8	\$475.00	\$3,800	\$1,900.00	\$1,900.00
62.	01150	12 INCH GATE VALVE	EA	8	\$1,000.00	\$8,000	\$4,000.00	\$4,000.00
63.	01160	HYDRANT ASSEMBLIES	EA	8	\$2,050.00	\$16,400	\$8,200.00	\$8,200.00
64.		RAILROAD TRACK CROSSING	LS	1	\$50,000.00	\$50,000	\$0.00	\$50,000
65.		TRENCH PROTECTION	LF	3900	\$1.50	\$5,850	\$3,100.50	\$2,749.50
<b>SUBTOTAL</b>						<b>\$321,850</b>	<b>\$143,235</b>	<b>\$178,616</b>

**SUBTOTAL OF CONSTRUCTION ITEMS**

<b>\$4,958,113</b>	<b>\$2,479,471</b>	<b>\$2,978,931</b>
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**DESIGN, ROW & CONTINGENCIES**

		RIGHT OF WAY (Residential)	SF	272,000	\$14.00	\$3,808,000	\$1,865,920.00	\$1,942,080.00
		TEMPORARY CONSTRUCTION EASEMENT	SF	34,000	\$1.00	\$34,000	\$16,660.00	\$17,340.00
		DESIGN AND CONST. MGMT. (25%)				\$1,239,528	\$619,868	\$744,733
		CONTINGENCIES (30%)				\$1,487,434	\$743,841	\$893,679
<b>SUBTOTAL INCLUDING ROW, DESIGN, AND CONTINGENCIES</b>						<b>\$6,568,962</b>	<b>\$3,246,289</b>	<b>\$3,597,832</b>

**TOTAL PRELIMINARY COST ESTIMATE**

<b>\$11,527,000</b>	<b>\$5,726,000</b>	<b>\$6,577,000</b>
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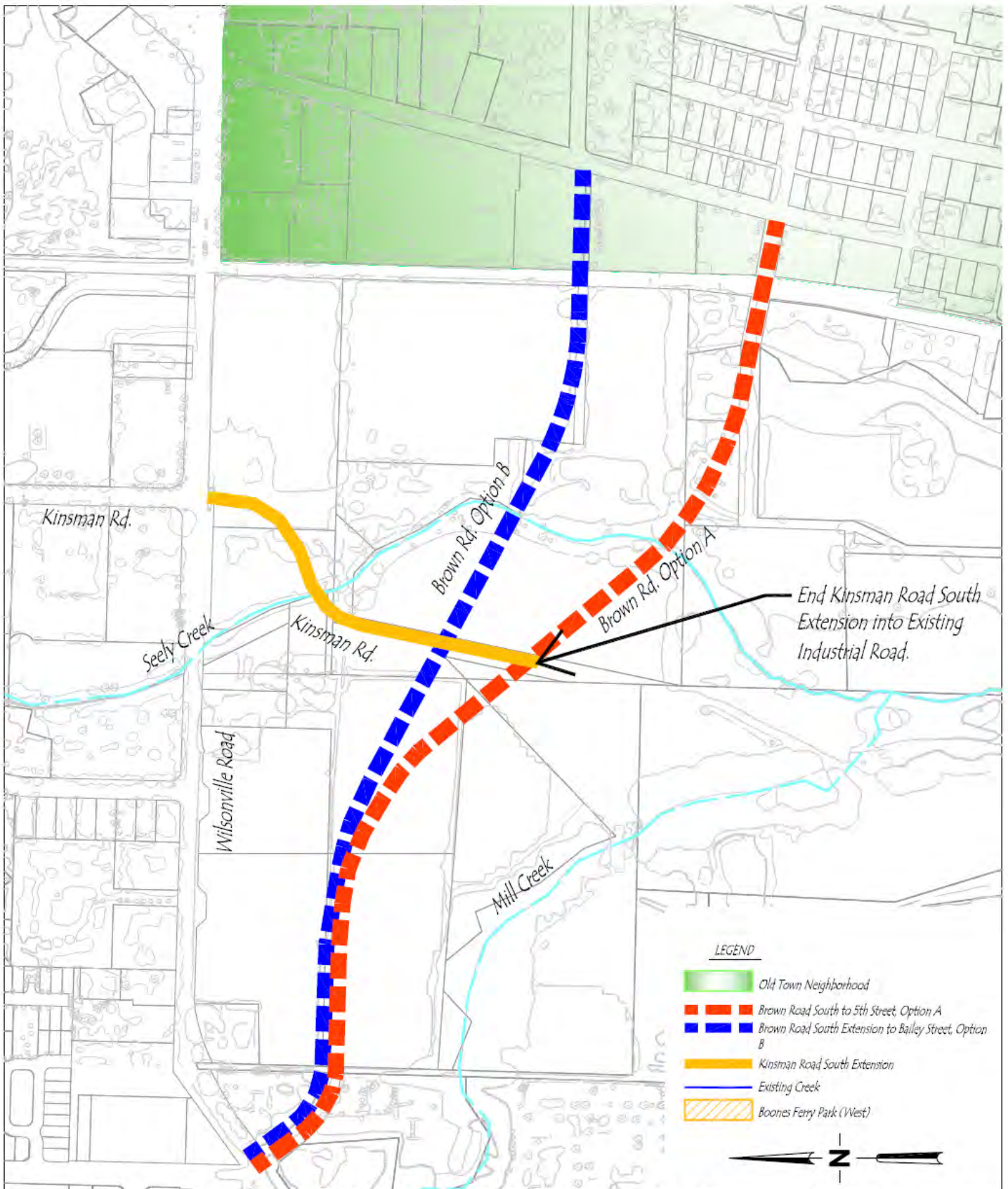
# **APPENDIX D**

## **Proposed Capital Projects Preliminary Cost Estimates June 2008**

### **Subsection E: Kinsman Road South Extension**







**OLD TOWN PRELIMINARY COST ESTIMATES**  
**OLD TOWN**  
**KINSMAN ROAD SOUTH EXTENSION**

<b>M&amp;S PROJECT #:</b> <u>14892</u>
<b>DRAWN BY:</b> <u>EH/MW</u>
<b>CHECKED BY:</b> <u>RP</u>
<b>DATE:</b> <u>06/16/08</u>
<b>SHEET NO.</b> <u>E1</u>

Scale: 1" = 500'

**SUBSECTION E2**

**PROJECT:** KINSMAN ROAD SOUTH EXTENSION  
FROM WILSONVILLE ROAD TO FUTURE BROWN ROAD  
MINOR COLLECTOR WITH NO PARKING

**OWNER:** CITY OF WILSONVILLE

**DATE:** MAY 6, 2008

**NOTE:**

BROWN ROAD (1) IS FROM WILSONVILLE RD TO 5TH  
BROWN ROAD (2) IS FROM WILSONVILLE RD TO BAILEY

**ENGINEER'S ESTIMATES**

				FROM WILSONVILLE ROAD TO FUTURE BROWN ROAD TOTAL LENGTH, 1,400 FT			WILSONVILLE RD TO BROWN RD (2) LENGTH, 960 FT	BROWN RD (2) TO BROWN RD (1) LENGTH, 440 FT
ITEM	SECTION NUMBER	DESCRIPTION	UNIT	QUANTITY	UNIT COST	TOTAL	69% OF TOTAL OR AS NOTED	31% OF TOTAL OR AS NOTED
<b>PART 00150 - CONTROL OF WORK</b>								
1.	00150	EXTENDED MAINTENANCE (2YR)	LS		\$2,000.00	\$2,000	\$2,000.00	\$2,000.00
<b>SUBTOTAL</b>						\$2,000	\$2,000	\$2,000
<b>PART 00200 - TEMPORARY FEATURES AND APPURTENANCES</b>								
2.	00210	MOBILIZATION	LS			\$269,448	\$180,187	\$99,360
3.	00225	TEMPORARY PROTECTION AND DIRECTION OF TRAFFIC	LS		\$8,000.00	\$8,000	\$5,520.00	\$2,480.00
4.	00225	TEMPORARY SIGNS	SF	320	\$17.00	\$5,440	\$3,753.60	\$1,686.40
5.	00225	TEMPORARY BARRICADES, TYPE III	FT	100	\$65.00	\$6,500.00	\$6,500.00	\$6,500.00
6.	00225	PORTABLE CHANGEABLE MESSAGE BORD SIGNS	DAY	120	\$200.00	\$24,000.00	\$24,000.00	\$24,000.00
7.	00225	FLAGGERS (TRAFFIC CONTROL LABOR)	HR	1600	\$40.00	\$64,000	\$64,000	\$64,000
<b>SUBTOTAL</b>						\$377,388	\$283,961	\$198,026
<b>EROSION CONTROL</b>								
8.	00280	TEMPORARY TYPE ORANGE PLASTIC MESH FENCE	LF	2800	\$2.50	\$7,000	\$4,830.00	\$2,170.00
9.	00280	CONSTRUCTION ENTRANCES	EA	2	\$1,145.00	\$2,290	\$2,290	\$2,290
10.	00280	INLET PROTECTION	EA	8	\$55.00	\$440	\$330.00	\$110.00
11.	00280	TIRE WASH FACILITY	EA	1	\$2,000.00	\$2,000	\$2,000	\$2,000
12.	00280	BIOFILTER BAGS	EA	84	\$14.50	\$1,218	\$841.00	\$377.00
13.	00280	SEDIMENT FENCE, UNSUPPORTED	LF	2800	\$3.00	\$8,400	\$5,796.00	\$2,604.00
14.	00280	TEMPORARY SEEDING	AC	1.5	\$2,530.00	\$3,795	\$2,618.55	\$1,176.45
15.	00280	SAWCUT EXISTING PAVEMENT	LF	200	\$2.00	\$400	\$276.00	\$124.00
16.	00280	REMOVAL OF SURFACINGS	SY	200	\$4.00	\$800	\$552.00	\$248.00
<b>SUBTOTAL</b>						\$26,343	\$19,533.55	\$11,099.45

**PART 00300 - ROADWORK**

17.	00310	REMOVAL OF STRUCTURES AND OBSTRUCTIONS	LS	1	\$10,000.00	\$10,000	\$6,900.00	\$3,100.00
18.	00320	CLEARING AND GRUBBING	LS	1	\$20,000.00	\$20,000	\$13,800.00	\$6,200.00
19.	00330	COMMON EXCAVATION	CY	8000	\$14.00	\$112,000	\$77,280.00	\$34,720.00
20.	00331	SUBGRADE STABILIZATION (IF APPLICABLE)	CY	1300	\$25.00	\$32,500	\$22,425.00	\$10,075.00
21.	00340	WATERING	LS	1	\$4,000.00	\$4,000	\$2,760.00	\$1,240.00
22.	00350	GEOSYNTHETIC INSTALLATION	SY	7780	\$1.00	\$7,780	\$5,368.20	\$2,411.80
<b>SUBTOTAL</b>						<b>\$186,280</b>	<b>\$128,533.20</b>	<b>\$57,746.80</b>

**PART 00400 - DRAINAGE AND SEWERS****STORM SEWERS**

23.	00470	STORM MANHOLE	EA	4	\$2,000.00	\$8,000	\$6,000.00	\$2,000.00
24.	00470	CONCRETE INLETS, TYPE CG-30	EA	8	\$1,300.00	\$10,400	\$7,800.00	\$2,600.00
25.	00470	10" STORM PIPE	LF	200	\$40.00	\$8,000	\$5,520.00	\$2,480.00
26.	00470	24" STORM PIPE	LF	1400	\$60.00	\$84,000	\$57,960.00	\$26,040.00
27.	00470	6'x4' BOX CULVERT WITH 8' HEAD WALLS	EA	1	\$150,000.00	\$150,000	\$150,000.00	\$0
28.		TRENCH PROTECTION	LF	1400	\$2.00	\$2,800	\$1,932.00	\$868.00
<b>SUBTOTAL</b>						<b>\$263,200</b>	<b>\$229,212</b>	<b>\$33,988</b>

**SANITARY SEWERS**

29.	00445	8 INCH SANITARY SEWER PIPE, 10 FT DEPTH	LF	1400	\$85.00	\$119,000	\$82,110.00	\$36,890.00
30.		SANITARY MANHOLE	EA	4	\$2,000.00	\$8,000	\$6,000.00	\$2,000.00
31.		TRENCH PROTECTION	LF	1400	\$4.00	\$5,600	\$3,864.00	\$1,736.00
<b>SUBTOTAL</b>						<b>\$132,600</b>	<b>\$91,974</b>	<b>\$40,626</b>

**UTILITY CONDUITS & VAULTS**

32.	00445	2 INCH PVC PIPE, 5 FT DEPTH	LF	5200	\$2.00	\$10,400	\$7,176.00	\$3,224.00
33.	00445	4 INCH PVC PIPE, 5 FT DEPTH	LF	9600	\$2.50	\$24,000	\$16,560.00	\$7,440.00
34.		UTILITY TRENCH EXCAVATION	LF	1800	\$30.00	\$54,000	\$37,260.00	\$16,740.00
35.	00470	UTILITY VAULT, 12' X 6' X 8'	EA	2	\$4,600.00	\$9,200	\$4,600.00	\$4,600.00
36.	00470	UTILITY VAULT, 2' X 3'	EA	4	\$800.00	\$3,200	\$2,400.00	\$800.00
<b>SUBTOTAL</b>						<b>\$100,800</b>	<b>\$67,996</b>	<b>\$32,804</b>

**PART 00600 - BASES**

37.	00640	AGGREGATE BASE	CY	1762	\$40.00	\$70,480	\$48,631.20	\$21,848.80
<b>SUBTOTAL</b>						<b>\$70,480</b>	<b>\$48,631</b>	<b>\$21,849</b>

**PART 00700 - WEARING SURFACES**

38.	00744	ASPHALT APPROACHES	CY	420	\$650.00	\$273,000	\$188,370.00	\$84,630.00
39.	00759	CURB AND GUTTER CONCRETE CURBS	LF	2960	\$14.00	\$41,440	\$28,593.60	\$12,846.40
40.	00759	CONCRETE DRIVEWAYS	SY	228	\$75.00	\$17,100	\$11,799.00	\$5,301.00
41.	00759	CONCRETE WALKS	SY	1560	\$45.00	\$70,200	\$48,438.00	\$21,762.00
42.	00755	7" PCC PAVEMENT	SF	70000	\$6.00	\$420,000	\$289,800.00	\$130,200.00
<b>SUBTOTAL</b>						<b>\$821,740</b>	<b>\$567,001</b>	<b>\$254,739</b>

**PART 00800 - PERMANENT TRAFFIC SAFETY AND GUIDANCE DEVICES**

43.	00850	PAVEMENT LEGEND, TYPE A: ARROWS	EA	8	\$100.00	\$800	\$600.00	\$200.00
44.	00850	PAVEMENT LEGEND, TYPE A: BICYCLE LANE SYMBOLS	EA	8	\$125.00	\$1,000	\$750.00	\$250.00
45.	00861	PAINTED PERMANENT PAVEMENT STRIPING	LF	8800	\$0.10	\$880	\$607.20	\$272.80
46.	00863	STRIPING AND STRIPE REMOVAL MOBILIZATION	EA	1	\$200.00	\$200	\$200.00	\$200.00
47.	00850	PVMNT LEGEND; TYPE B "ONLY"	EA	2	\$275.00	\$550	\$275.00	\$275.00
48.	00860	BI DIRECTIONAL TYPE I YELLOW MARKERS	EA	145	\$4.00	\$580	\$400.00	\$180.00
49.	00860	WHITE TYPE II MARKERS	EA	80	\$3.00	\$240	\$165.00	\$75.00
<b>SUBTOTAL</b>						<b>\$4,250</b>	<b>\$2,997</b>	<b>\$1,453</b>

**PART 00900 - PERMANENT TRAFFIC CONTROL AND ILLUMINATION SYSTEMS**

50.	00930	SIGN AND LUMINAIRE SUPPORTS	LS	1	\$2,000.00	\$2,000	\$2,000	\$2,000
51.	00990	TRAFFIC SIGNAL INSTALLATION	EA	2	\$350,000.00	\$700,000	\$350,000.00	\$350,000.00
52.	00960	COBRA HEAD LUMINAIRES, POLES, FOUNDATIONS, JUNCTION	EA	20	\$3,400.00	\$68,000	\$47,600.00	\$20,400.00
53.	00990	ILLUMINATION SERVICE CABINET AND EQUIPMENT	EA	2	\$7,500.00	\$15,000	\$7,500.00	\$7,500.00
<b>SUBTOTAL</b>						<b>\$785,000</b>	<b>\$407,100</b>	<b>\$379,900</b>

**PART 01000 - RIGHT OF WAY DEVELOPMENT AND CONTROL**

54.	01040	LANDSCAPING	LS	1	\$84,000.00	\$84,000	\$57,960.00	\$26,040.00
55.	01040	WETLAND MITIGATION	AC	0.15	\$5,000.00	\$750	\$750.00	\$0
<b>SUBTOTAL</b>						<b>\$84,750</b>	<b>\$58,710</b>	<b>\$26,040</b>

**PART 01100 - WATER SUPPLY SYSTEMS**

56.	01120	IRRIGATION	LS	1	\$24,000.00	\$24,000	\$16,560.00	\$7,440.00
57.	01170	2 INCH POTABLE WATER PIPE, FITTINGS AND COUPLINGS	LF	280	\$30.00	\$8,400	\$5,796.00	\$2,604.00
58.	01170	12 INCH POTABLE WATER PIPE, FITTINGS AND COUPLINGS	LF	1340	\$49.00	\$65,660	\$45,305.40	\$20,354.60
59.	01150	6 INCH GATE VALVE	EA	3	\$475.00	\$1,425	\$950.00	\$475.00
60.	01150	12 INCH GATE VALVE	EA	3	\$1,000.00	\$3,000	\$2,000.00	\$1,000.00
61.	01160	HYDRANT ASSEMBLIES	EA	3	\$2,050.00	\$6,150	\$4,100.00	\$2,050.00
62.		TRENCH PROTECTION	LF	1640	\$1.50	\$2,460	\$1,697.40	\$762.60
<b>SUBTOTAL</b>						<b>\$111,095</b>	<b>\$76,409</b>	<b>\$34,686</b>

**SUBTOTAL OF CONSTRUCTION ITEMS**

<b>\$2,965,926</b>	<b>\$1,984,057</b>	<b>\$1,094,958</b>
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**DESIGN, ROW & CONTINGENCIES**

		RIGHT OF WAY (Residential)	SF	49,780	\$14.00	\$696,920	\$696,920.00	\$0.00
		RIGHT OF WAY (Residential)	SF	28,350	\$14.00	\$396,900	\$0.00	\$396,900.00
		TEMPORARY CONSTRUCTION EASEMENT	SF	19,540	\$1.00	\$19,540	\$19,540.00	\$0.00
		TEMPORARY CONSTRUCTION EASEMENT	SF	1,000	\$1.00	\$1,000	\$0.00	\$1,000.00
		DESIGN AND CONST. MGMT. (25%)				\$741,481	\$496,014	\$273,739
		CONTINGENCIES (30%)				\$889,778	\$595,217	\$328,487
<b>SUBTOTAL INCLUDING ROW, DESIGN, AND CONTINGENCIES</b>						<b>\$2,745,619</b>	<b>\$1,807,691</b>	<b>\$1,000,127</b>

**TOTAL PRELIMINARY COST ESTIMATE**

<b>\$5,712,000</b>	<b>\$3,792,000</b>	<b>\$2,095,000</b>
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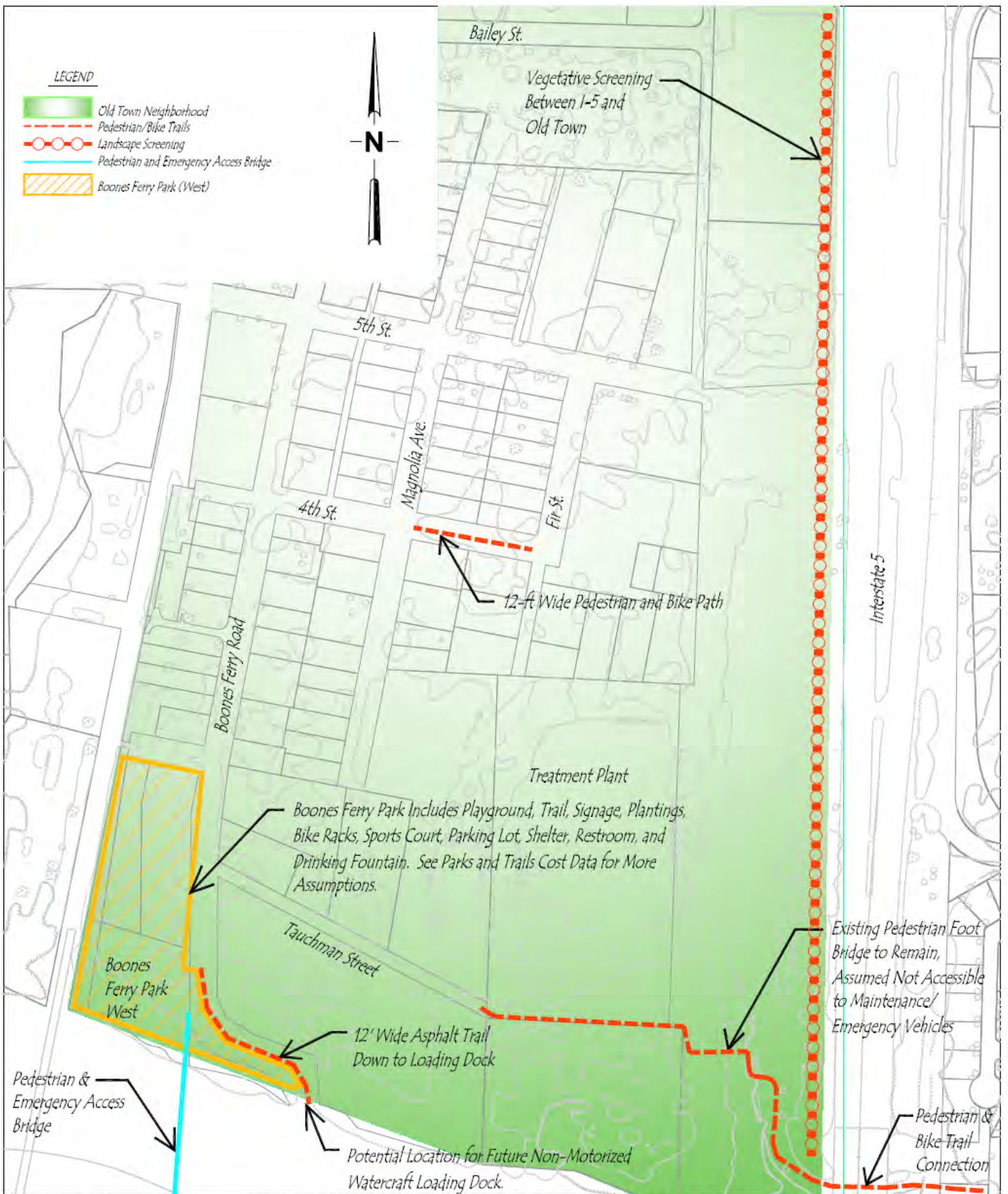
# **APPENDIX D**

## **Proposed Capital Projects Preliminary Cost Estimates June 2008**

### **Subsection F: Park and Trails Improvements**







**OLD TOWN PRELIMINARY COST ESTIMATES**

**OLD TOWN  
PARK AND TRAILS**

M&S PROJECT #: 14892

DRAWN BY: EH/MW

CHECKED BY: RP

DATE: 06/16/08

SHEET NO. F1

Scale: 1" = 300'



**SUBSECTION F2**

**PROJECT:**

**PARKS AND TRAILS - PRELIMINARY COST ESTIMATES**

**OWNER:**

**CITY OF WILSONVILLE**

**DATE: MAY 6, 2008**

				<b>ENGINEER'S ESTIMATES</b>									
ITEM	DESCRIPTION	UNIT PRICE	UNIT	<b>TOTAL</b>									
				QUANTITY	BOONES FERRY PARK, SEE PARKS AND TRAILS COST DATA FOR ASSUMPTIONS	QUANTITY	VEGETATIVE SCREENING BETWEEN I-5 AND OLD TOWN, TREE EVERY 10-FEET, ESTIMATED 2400-FEET FROM BAILEY DOWN TO I-5 BRIDGE	QUANTITY	SIDEWALK/TRAIL IN 4TH STREET RIGHT-OF-WAY BETWEEN MAGNOLIA AND FIR, SEE PARKS AND TRAILS COST DATA FOR ASSUMPTIONS	QUANTITY	PEDESTRIAN/BIKE TRAIL CONNECTION FROM TAUCHMAN TO EAST SIDE OF FREEWAY	QUANTITY	BOAT DOCK, SELF ADJUSTING BOARDING FLOAT SYSTEM AND RAMP, INCLUDING PATH TO RAMP
<b>CONSTRUCTION</b>													
1.	Mobilization		L.S.		\$50,204.00		\$8,610.00		\$1,148.40		\$5,509.00		\$11,183.00
2.	Clearing and Grubbing		L.S.		\$1,000.00		\$500.00		\$200.00		\$200.00		\$500.00
3.	Unclassified Excavation	\$14.00	C.Y.	100	\$1,400.00	100	\$1,400.00	10	\$140.00	10	\$140.00	20	\$280.00
4.	Sediment Fence, Unsupported	\$3.00	L.F.	1000	\$3,000.00	2400	\$7,200.00	323	\$969.00	1300	\$3,900.00	100	\$300.00
5.	Permanent Seeding, Complete		L.S.				\$5,000.00		\$100.00		\$100.00		\$250.00
6.	Landscaping, Complete		L.S.						\$2,000.00		\$2,500.00		\$500.00
<b>TOTAL CONSTRUCTION</b>					\$55,604.00		\$22,710.00		\$4,557.40		\$12,349.00		\$13,013.00
<b>SPECIAL ITEMS</b>													
1.	Park Improvements	\$128,000.00	AC	3.88	\$496,640.00								
2.	Tree Screening	\$300.00	EA			240	\$72,000.00						
3.	12' Wide Asphalt Path/Trail	\$25.00	LF					323	\$8,075.00	1380	\$34,500.00	400	\$10,000.00
4.	Alternate 12' Wide Asphalt Pat/Trail/Veh. Access	\$25.00	LF							550	\$13,750.00		
5.	Boat Dock	\$100,000.00	LS										\$100,000.00
<b>TOTAL OF SPECIAL ITEMS</b>					\$496,640.00		\$72,000.00		\$8,075.00		\$48,250.00		\$110,000.00
<b>DESIGN, CONSTRUCTION AND CONTINGENCY</b>													
1.	Design & Construction Management	30%			\$165,673.20		\$28,413.00		\$3,789.72		\$18,179.70		\$33,000.00
2.	Contingency	40%			\$220,897.60		\$37,884.00		\$5,052.96		\$24,239.60		\$44,000.00
<b>TOTAL CONSTRUCTION WITH DESIGN, CONSTRUCTION, AND CONTINGENCY</b>					\$939,000.00		\$161,000.00		\$21,000.00		\$103,000.00		\$200,000.00

**SUBSECTION F3**  
**PARKS AND TRAILS COST DATA**  
 Data Obtained from MacKay and Sposito's Landscaping Department

**Amenities Cost Breakdown**  
**Local = 3 Acre Neighborhood Park (Medium Development Level)**

Cost	Description
\$ 60,000	Playground
\$ 3,500	Benches (4) on conc. (\$875 Each)
\$ 3,000	Tables (2) @ 1500/EA
\$ 40,000	Trail asphalt 10' wide \$20/lf - 2000'
\$ 2,000	Signage 1D sign (1)
\$ 15,000	Seeding \$0.12/SF
\$ 32,500	Shrubs/groundcover (planted) \$2.50/SF 10% ofsite
\$ 11,200	Perimeter fence / 4' High - \$8/LF (1400')
\$ 16,250	Irrigation - \$1.25/SF (1300 SF)
\$ 4,000	Power
\$ 10,000	Water
\$ 25,000	Grading/earthwork (6" deep @ 3AC.)
\$ 10,000	Fine Grading - \$0.60/SY
\$ 18,000	Trees \$300/Tree - Assume 60
\$ 200	Bike Racks
\$ 700	Trash
\$ 251,350	Total
\$ 84,000	Per Acre

**Community Park = 6 Acre**

Cost	Description
\$ 80,000	Playground
\$ 7,000	Benches (8) on conc. (\$875 Each)
\$ 6,000	Tables (4) @ 1500/EA
\$ 60,000	Trail asphalt 10' wide \$20/lf - 3000'
\$ 4,000	Signage 1D sign (2)
\$ 30,000	Seeding \$0.12/SF
\$ 65,000	Shrubs/groundcover (planted) \$2.50/SF 10% ofsite 26000 SF
\$ 24,000	Perimeter fence / 4' High - \$8/LF (1400')
\$ 32,500	Irrigation - \$1.25/SF (1300 SF)
\$ 4,000	Power
\$ 13,000	Water
\$ 50,000	Grading/earthwork (6" deep @ 3AC.)
\$ 20,000	Fine Grading - \$0.60/SY
\$ 36,000	Trees \$300/Tree - Assume 120
\$ 1,000	Bike Racks
\$ 1,400	Trash
\$ 35,000	Sports court \$22/SY Acrylic colored
\$ 21,600	Parking Lot
\$ 60,000	Shelter (20'x30')
\$ 200,000	Restroom
\$ 13,000	Backstop (2/EA)
\$ 3,500	Drinking Fountain
\$ 767,000	Total
\$ 128,000	Per Acre

**Trail Cost**

\$ 22	12' Wide Path/Maintenance Vehicle Path (LF)
\$ 0.25	Total Seeding and Minor Grading (cost/SF) or \$3/LF



# **APPENDIX D**

## **Proposed Capital Projects Preliminary Cost Estimates June 2008**

### **Subsection G: Miscellaneous Supporting Documents for Cost Estimates**



**MISCELLANEOUS SUPPORTING DOCUMENTS FOR COST ESTIMATES**

The following list the City staff recommendation for utility improvements for the Old Town Neighborhood.

**Storm sewer main Information**

- Most of the existing storm sewer lines are currently 27 years old concrete pipes.
- Boones Ferry Road
  - Fred Meyer to add 24" storm
  - From Bailey Street to Fifth Street, existing 24" N-12 pipes installed in 2002. Assumed line may need to be replaced due to storm realignment and/or adjustment.
  - From Bailey Street to Fifth Street, existing 18" CSP pipes installed in 1992. Replace per City staff.
  - From Fifth Street to Boones Ferry Park, existing 18"-24" concrete pipes installed in 1981. Replace per City staff.
- Magnolia Avenue
  - North of Fourth Street, existing 12"-24" concrete pipe installed in 1981. Assumed line may need to be replaced due to storm realignment and/or adjustment.
  - South of Fourth Street, existing 12" concrete pipe installed in 1986. Assumed line may need to be replaced due to storm realignment and/or adjustment.
- Fourth Street and Fifth Street
  - Existing 12"-24" concrete pipe installed in 1981. Assumed line may need to be replaced due to storm realignment and/or adjustment.

**Sanitary sewer main Information**

- Most of the existing sanitary sewer lines are 32 to 36 years old.
- Sanitary sewer lines would need to be TV, but assumed all lines to be replaced for estimating purposes per City staff recommendation.

**Water main Information**

- Most of the 14" cast iron pipes are 37 years old.
- Boones Ferry Road and Fifth Street
  - Existing 14" cast iron pipes. Upgrade line to 18" ductile iron pipe per City staff recommendation.
- Magnolia Avenue, Fir Avenue and Fourth Street.
  - Existing 6"-8" cast iron pipes. Upgrade line to 8" ductile iron pipe per City staff recommendation.